

RICHLAND COUNTY COUNCIL

SOUTH CAROLINA

TRANSPORTATION AD HOC COMMITTEE

March 5th, 2019

1:00 PM

County Council Chambers

1. Call to Order
2. Approval of Minutes (Pages 1-13)
3. Adoption of the Agenda
4. Approval of the Executive Summary & Recommendations for:
(Pages 14-42)
 - 1) Trenholm Acres/ Newcastle Neighborhood Improvement Project
(Pages 14-32)
 - 2) Shop Road Extension Phase 2 (Pages 33-36)
 - 3) Blythewood Area Improvements (McNulty Street Improvements)
(Pages 37-42)
5. Approval of Percival Road Sidewalk Service Modification
(Pages 43-50)
6. Approval of Decker BLVD/Woodfield Park Neighborhood
Improvement Project landscaped medians and driveway closures
(Pages 51-53)
7. Approval of Blythewood Road Widening Shared Use Path Maintenance
Agreement with SCDOT (Pages 54-55)
8. Approval of Blythewood Area Improvements: Town of Blythewood
Priorities Resolution (Pages 56-70)
9. Approval of Atlas Road Widening SCE&G Utility Agreement
(Pages 71-75)
10. Approval of Shop Road Widening Termini Change from South Beltline
to Mauning Drive (Pages 76-79)
11. Approval of Service Orders: (Pages 80-113)
 - 1) Clemson/Sparkleberry Intersection (Pages 80-103)
 - 2) Broad River Road Widening (Pages 104-113)
12. Approval of Award Letter Recommending to Award Bid
(Pages 114-122)
 - 1) Broad River Neighborhood Improvement Project (Pages 114-116)
 - 2) Dirt Road Package I (Pages 117-119)
 - 3) Southeastern Neighborhood Improvement Project
(Pages 120-122)



Committee Members

Calvin "Chip" Jackson,
Chair District Nine

Paul Livingston
District Four

Jim Manning
District Eight

Dalhi Myers
District Ten

Chakisse Newton
District Eleven

RICHLAND COUNTY COUNCIL
S O U T H C A R O L I N A

TRANSPORTATION DEPARTMENT

March 5, 2019
1:00 PM
County Council Chambers

13. Approval of Calhoun Road Diet Executive Summary and recommendations (Pages 123-151)
14. Approval of Jushi Letter Request for Extension (Page 152)
15. Discussion: Project Status Update (Pages 153-159)
16. Adjournment



Committee Members

Calvin "Chip" Jackson, Chair
District Nine

Paul Livingston
District Four

Jim Manning
District Eight

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District Ten

Chakisse Newton
District Eleven



Richland County Council
Transportation Ad Hoc Committee
December 4, 2018 – 1:00 PM
Council Chambers
2020 Hampton Street, Columbia 29204

COMMITTEE MEMBERS PRESENT: Calvin “Chip” Jackson, Chair; Yvonne McBride, Bill Malinowski, Norman Jackson and Paul Livingston

OTHERS PRESENT: Michelle Onley, John Thompson, Eden Logan, Bryant Davis, Mohammed Al-Tofan, Kimberly Toney, and Nathaniel Miller

1. **Call to Order** – Mr. C. Jackson called the meeting to order at approximately 1:00 PM.
2. **Adoption of the Agenda** – Mr. Livingston moved, seconded by Mr. Malinowski, to adopt the agenda as published.

In Favor: Malinowski, C. Jackson, N. Jackson, Livingston and McBride

The vote in favor was unanimous.

3. **Approval of Resolution Authorizing the Issuance and Sale of Not to Exceed \$158M for the General Obligation Bond Anticipation Notes** – Mr. C. Jackson stated we discussed this at our last meeting, and it was not an action item. One of the main reasons for today’s meeting is to take action, and have further discussion regarding the figures.

Dr. Thompson stated there is a breakdown of how they came up with \$158M figure on pp. 4 of the agenda.

Mr. Malinowski inquired about the third item on the list, \$20M to be reimbursed for the time period May 15 – June 30, 2018. He inquired, for clarification, if we have expenditures this long ago that have not been paid.

Dr. Thompson stated they were waiting for permission, in regards to the SCDOR situation, to use the funds. They used the funds from the Penny Sales Tax revenue. Now is an opportunity to use the BAN funds to reimburse the Penny Sales Tax revenues.

Mr. Malinowski stated, for clarification, we have already paid all of the bills out of the Penny Sales Tax funds and now we are going to go borrow money to replenish Penny Sales Tax funds.

Dr. Thompson stated we already have the BAN money. The BAN was released...

Mr. Malinowski stated the BAN money has to be paid back at some point. So, if we have already paid the bills, and we have the BAN, why do we take it and continue to pay interest for a long period of time. Why not just

put the BAN money back to pay off the BAN, instead of incurring additional interest?

Mr. Gomeau stated the BAN money has already incurred the expense. The \$250 Million BAN was issued last February, so we are accruing the interest expense everyday up until the BANs are reimbursed. This is just a formality to put the money back in the Penny Fund. It is not going to cost us anymore interest. We are going to pay interest on the \$250.

Mr. Malinowski stated he thought the interest continued as long as we keep it.

Mr. Gomeau stated the interest will continue.

Mr. Malinowski inquired if it will not be reduced by reimbursing that.

Mr. Gomeau stated not until we rebate the bonds in February.

Mr. Livingston stated, as he recalls, in December 2017, we got a list of project from the Program Development Team, which was based on the \$250M BAN we are looking at now. Let's assume we do the \$150M, that is being recommended. If it is true that we can get to those program, how are we going to decide which of those projects does not go forward. He stated he can see Council members arguing about details. One thing that concerns him, if we end up getting those projects moving, as was illustrated in December 2017, that may create a problem for us. Secondly, what is the downside of doing the \$250M, in case you need it. And, what is the upside. He thinks the upside is you have a chance to get all the projects done, if you move them fast. In addition, you would get ahead of SCDOT on all their projects; therefore, we will not be stuck with higher costs, if we can get them done. With that said, remember the voters said you could do up to \$450M. He thinks it may be a little risky not to do it. His first question is, what is the cost and problem with going on ahead and doing the \$250M now, as opposed to the \$150M

Mr. Gomeau stated the logical answer to that is we are not going to be able to spend \$250M. You are going to incur interest costs on that \$250 again, just as you did this year. We are going to have \$100M in BAN. We are going to have about \$30M left in the Penny Fund in February. And, we are going to collect another \$65M from the Penny Fund during the year. We are going to have enough money to do Dr. Thompson's projects, and have a balance at the end of December 2019. In borrowing money, and not using it, as we did this year, causes people like the IRS to look at what you are doing with that money. You borrowed \$250M this year, and used very little of it. If you go ahead and do the same thing next year, and you do not use \$250, they are going to look at this thing. You are going out to an investment community asking for money, saying you are going to do projects worth \$250, and you are not doing them. Just from that point of view it is not a good idea. The basic problem is you are going to pay interest that you do not have to pay. You have enough money to do this with the plan that we have in place.

Mr. Livingston stated you would be gaining interest to offset the interest also.

Mr. Gomeau stated you cannot arbitrage the \$250. You get some interest from the Penny Fund, which we think we offset some of the cost of the \$150M, but you are not going to be able to get \$7M in interest earnings to pay off the bond interest. You cannot arbitrage those funds. We are going to be penalized this year, in terms of arbitrage.

Mr. Livingston stated he remembers bond counsel saying that, but, if, he remembers right that penalty was pretty small relative to interest vs. the arbitrage costs. Before this gets to Council, you can give me the difference, but he thought it was small, in terms of what it costs us when you are considering the arbitrage and the interest that you gain on it.

Mr. Gomeau stated it is a precarious practice to be doing that. The IRS is going to look at this, at some point, if you go year after year with arbitrage violations. Are you borrowing money just to make interest, or are you borrowing money to do project? The purpose of the arbitrage regulation, in the 70s, was to offset this. That was what communities were doing. They were borrowing money, investing it and not doing the projects. He stated it is not a good idea. It is going to affect the County's bond rating, at some point if they keep seeing you do this. It is going to have an impact on what we are doing here by taking up time paying money back that we should not have to borrow in the first place.

Mr. Livingston stated, for clarification, we are not considering the information we got about what we can do in 2017, when the Program Development Team gave the projects and the completion dates. What Mr. Gomeau is saying is that he does not think that can happen.

Dr. Thompson stated, based on the PDT's outlay documents, he presented to Administration a drawdown schedule. For the next fiscal year, we are anticipating they will spend a little over a \$100M. With other funds we have from the revenue sales, we will have sufficient monies to carry out all of the projects the PDT has identified.

Mr. Livingston stated, for clarification, Dr. Thompson is going off of something different from what he got in 2017, obviously.

Dr. Thompson stated, absolutely not. We are still looking at their outlays document. Keep in mind, the PDT updates this document on a quarterly basis.

Mr. Livingston stated he was going by the one that was done in December 2017, which spelled out the whole \$250M, for the period of time. Obviously if that has changed, then that is a different story.

Mr. N. Jackson inquired as to the average we spend on projects annually.

Dr. Thompson stated it is increasing. With the last fiscal year, it was a little over \$100M. We expect that for the next fiscal year. This current fiscal year, we are on track for approximately \$100M. We projected \$117M, but he thinks it is going to come at approximately \$90M. Then 2 years from now, we are looking at \$135M - \$145M.

Mr. N. Jackson inquired as to how much we take in with the Penny Tax Program annually.

Dr. Thompson stated \$65M, and it is increasing slightly.

Mr. N. Jackson inquired as to how much we have in the reserve.

Mr. Gomeau stated the last time he looked it was approximately \$29M. was in the Penny Fund Balance.

Mr. N. Jackson inquired if we spent any of the BAN funds.

Dr. Thompson responded in the affirmative.

Mr. Gomeau stated the numbers on the sheet for the payback is the money we are taking out of the BAN.

Mr. N. Jackson stated, for clarification, we have a balance of \$29M, and we are bringing \$65M in annually.

Mr. Gomeau stated we will have an additional \$100M in BAN funds in February 2019. It will give you the

opportunity to look at what you do over 2019. When they come due in February 2020, you can make a decision to bond the \$100M, if you feel you want to do that, at that point, if you have cash needs. If you do not have cash needs, that will give you another year under your belt to look at how fast you are progressing. If you start to progress faster, go ahead and issue the \$100M in bonds, and you can pay those back over 8 – 10 years. If you give you a window to look at how much you are doing. If you are doing that consistently, then you can go out and borrow the money. If you do not need to borrow the money, if you have enough available, then you will not to do it.

Mr. N. Jackson stated that is his concern, to see if eventually we are going need to borrow, or just leave it alone.

Mr. Gomeau stated you will have that ability in early 2020 to look at where you are and make a decision on bonding, at that point.

Ms. McBride stated, in case we need more than the \$158M that has been proposed, what happens then. We have spent all those funds, and have new projects that need to be constructed, and we do not have the money there. Do we have to go back and get the bonds?

Dr. Gomeau stated that is a management decision. Dr. Thompson can look at this schedule, and he can adjust his schedule depending on how he sees the projects coming due, how much cash will be needed, and whether or not he wants to start one, if he does not have the available cash. It is just a simple project financing method that he will use. Dr. Thompson will not put us in a deficit. He will put us in a position where we can keep paying the bills, and if he needs more money, he can go back for the \$100M.

Ms. McBride stated so we have to go back to the \$100M, and we are going to go back at a higher interest rate.

Mr. Gomeau stated you get a low interest rate on the Anticipation Note because it is only for one year. If you bond, it is going to be a little bit higher. It is still not high. You are talking about 3% – 3.5%. It is not going to be onerous for \$100M. It will cost you \$3 - \$4M to borrow that.

Ms. McBride stated, it was her understanding, that based on the numbers that we have and the proposed plans, we have adequate funding with the proposed amount of \$158M.

Dr. Thompson stated for the specified timeframe.

Ms. McBride inquired if that has been agreed upon by all parties.

Dr. Thompson stated those are the numbers they got from the PDT.

Mr. Malinowski inquired about the approximate cost of the arbitrage penalty we are going to have to pay.

Mr. Gomeau stated the bond attorneys are working on that. We will not know until it gets closer to the redemption date.

Mr. Malinowski inquired about a ballpark figure...\$5M or \$1M.

Mr. Gomeau stated it is done by arbitrage company that does this. One of the top auditing firms does that calculation for us. It depends on how we spent the money down in a time period in between, and how much more interest we earned than we actually paid out. It will not be significant, but it is a penalty and it flags the IRS, which is his main concern. It does send a signal out that we are doing these things. We are trying to

make money off of money that we are borrowing, and that is not allowed.

Mr. Livingston stated, he was under the impression, if we did not do the whole \$250M, we lose the ability to issue the remaining \$92M later.

Mr. Gomeau stated bond counsel has advised that we will be limited to the \$100M, but the \$100M is a lot of money.

Mr. Livingston stated, so his assumption is true, that if we only issue \$158M, if we need \$92M, we can no longer issue that later on.

Mr. Gomeau stated you can issue \$100M, one time, or BANs rolling over every year. We have looked at the cash flow schedule, and this is a doable way to do this. You do not need to go ahead and keep borrowing excess amounts of money to just leave there, and not use, in the hopes that you might get a fast spurt in the schedule. This is good management practice for projects like this. He is confident, looking at Dr. Thompson's numbers, that he can manage this thing to the end without getting into any kind of financial difficulty.

Mr. Livingston stated he is concerned about that too, but, at the same time, he is concerned about not holding up the projects, if there is a possibility they can continue on. One thing different too, he thinks we voted, this time, to make sure we spend bond proceeds first. So, that ought to speed up spending bond money too because you are not spending your collectibles. With that in mind, that had him wondering whether or not it there was a possibility of using the whole amount. He stated he shares Mr. Gomeau's concern, but let's think about the concern of needing it, and not having it too.

Mr. N. Jackson stated, since the passing of the gasoline tax, there is a lot more highway projects that have come on board, that is affecting construction companies. There is so much to do now. Before, the Penny Tax was the only source of money for construction, and there was a lot of competition. Do you think, with the Penny Tax projects and programs, and now with so much money available, with the passage of the gasoline tax, and these new projects, and so much construction going on, it will drive prices down or create a shortage of materials, and drive prices up.

Dr. Thompson stated they need to look into that.

Mr. N. Jackson stated he just wanted them to be aware, since the Legislators passed the gasoline tax, and there is so many projects to be done. There is a lot of money available, and the same people we are using, the State is using also.

Dr. Thompson stated, purely from an economic perspective, we expect if the demand continues to go up, then prices will go up.

Mr. N. Jackson stated, when there was less money, there was stiff competition. Now there is so much money, people are turning down jobs.

Mr. Gomeau stated we are blip on the petrol chemical industry though. They are ratcheting up. They have been ratcheting up for the last 4 or 5 years. He does not think we will see a significant difference. We will see some because some are local contractors, but overall we will not see that much difference in the ability to get people to do the work. The price may go up.

Mr. N. Jackson stated, in the State, there was no money for the gasoline tax for 31 years, and highway construction had almost ceased in South Carolina. The Penny Tax was the only source of highway

construction that was happening, until recently. Last year, the Legislators increased the gasoline tax. Now we have this new money, and you have all these roads that we were doing for the State, and being reimbursed later, through the STIP. Now, you have so much work, and some of the contractors may not have enough staff to handle all the work there is.

Mr. Gomeau stated that is assuming the State can do that kind of start-up. He does not know about South Carolina, but most states do not have the ability to start-up that quickly.

Mr. N. Jackson stated it is not about the start-up, but they have the funds. It is just something to consider in your projections.

Mr. Malinowski stated he needs more clarification. The \$250M BAN that was issued, and you are saying we have the money for now, we want to reimburse for our projects that we have already done. And, looking at p. 4, there is 4 items there; #2, #3, and #4 are past items. If you total those up, that is the \$157M. So, that \$157M will come from the initial \$250M BAN, correct?

Mr. Gomeau stated we are spending it out of that. That is why we are paying back \$158M. All we are asking for in the new BAN is \$158M, which incorporates that into it.

Mr. Malinowski stated, for clarification, Mr. Gomeau is saying we are taking \$57M out of the original BAN, and we are still putting \$57M into the upcoming BAN.

Mr. Gomeau stated we are paying the difference. In other words, we do not have enough to pay back the \$250M, so we are getting more money, to include with it, to pay the \$250M back. We are spending money from the \$250M. We have to reimburse the whole \$250M in February.

Mr. Malinowski stated, so that is why you have the \$57M in the new one.

Mr. Gomeau stated we need to come up with the \$250M to rebate the bonds.

Mr. Livingston stated all of his concerns is based on the document that he received in December 2017. That document clearly spells out the projects, even by districts, in terms of, what and how we would spend the \$250M. If he remembers correctly, if we committed to do the bond, we would run out of funding in the first quarter of October 2018. If that is still not the case, his point is irrelevant, but if it is, he needs to know what the list that replaced this to say we do not need the \$250M; there are less projects, or we are doing something different. There ought to be a new list to explain to him why we do not need the \$250M, and explain to him what is not going to make it on this list.

Dr. Thompson stated they will provide him with the list. Again, he pointed to the outlays document that the PDT projects and provides on a quarterly basis. That list clearly articulates all the projects they will do for that particular year, and all the way until the end of the program.

Mr. C. Jackson stated he thinks he is clear, if we only go out for another year of \$158M, then in 2020, if we determine that there is not enough money to do what needs to be done, we would not be able to get another one-year BAN, but we would be able to borrow money using the bond. Is that correct?

Mr. Gomeau stated you could have another \$100M BAN, but it would give you the opportunity to bond the \$100M. If you looked at Dr. Thompson and PDTs schedule, and saw that everything would work, you could borrow \$100M in bonds, pay it off over 8 – 10 years, to match up with the money that you need for the projects.

Mr. C. Jackson stated, the reason he is asking is because the length of the Penny funds coming in, the termination date is 2022, correct?

Mr. Gomeau stated it is 22 years from 2012, or whenever you approved the ordinance.

Mr. C. Jackson stated, for clarification, or the maximum amount of money being \$1.07 Billion. If we max out on the money, but we have not maxed out on the completion of the projects, we make the determination that there will be a need for additional funds, then the Council, or committee, will have to decide whether we do a \$100M period, or we can do more than that by going with a bond.

Mr. Gomeau stated you should be able to do it in the future, if you have decided you are out of the Penny Fund completely. The 2 things you need to look at is, do you want to continue the program on beyond the money you actually receive, and you need to look at having a maintenance program that goes with this program so your \$1 Billion does not get lost. You cannot just put \$1 Billion into what they are doing and not a commitment to a maintenance program, which is going to be expensive. Either way, those 2 things you would have to look at. Do you want to keep going, and you should keep going? You should establish a long-term road program, even without the Penny Fund. You should have the maintenance program that goes with that, so you are not reconstructing roads after they have fallen apart in 15 years. You can keep them maintained for a long period of time.

Mr. C. Jackson stated, it is clear to him, at least from what he has read, that we have more projects on the list than we have funds to complete them all, if we were to try to go down the list as it currently exists. One effort was that the PDT provided us with some recommendations on some modifications on a couple projects, at the end of the list, that would not impact those areas significantly, but would also allow us to complete everything on the list. There were a couple scenarios the PDT presented. He thinks that is the same list, that Mr. Livingston was referring to, where there was a discussion that if we did not borrow the money we would run out of money by a particular date, possibly June/July, and would not be able to continue. He wants to be real clear before we vote, that, if in fact, we are going to try to complete everything on the approved referendum list, his understanding, today, is that we do not have enough money to do that. There is the possibility, next year, recognizing that fact, to bond additional funds to be able to cover that, as well as, maintenance money that Mr. Gomeau referred to. If that is the case, for him all hope is not lost, in order to make sure that we can complete the projects on the list.

Mr. Gomeau stated he agrees completely. He thinks somebody owes the public the ability to try to complete the projects they were told would be completed when they approved the bond resolution for the Penny Fund. If we cannot do it, we need to come up with a reasonable explanation of why we cannot do it. He does not think you can let that thing lie dormant, and not explain it to the public.

Mr. C. Jackson stated he agrees. The only mistake he does not want us to make is that because we do not borrow enough money, we do not complete the projects because we thought we had enough money, and find out later we do not have enough and we cannot go back and get it the second time around. His concern is, when we get to the point of needing more, and realizing when need more, at that point we cannot get more and we have missed the window.

Mr. N. Jackson stated we are limited to the amount of money we can borrow.

Mr. C. Jackson stated that is correct.

Mr. Malinowski stated the amount, or number, of projects to be completed, while they may not all be done, if the funds are not here, they are still on the list to be done in the future.

Dr. Thompson responded in the affirmative.

Mr. C. Jackson stated his question was how would we fund those.

Mr. Gomeau stated you can establish another road program. You can bond for that in the future. Lots of communities do that. That is not an unusual thing to do because it is a large expense.

Mr. Livingston stated his other main concern is he wants us to get the projects done as quickly as we can. He does not want anything to hold that up because he knows that is going to cost us more in the long run. The voters voted for us to bond the money, so we can get them done quickly.

Ms. McBride stated, this is somewhat different from the budget component that we are discussing now, but why is it we cannot get all of these other projects done. She inquired if there are not enough workers to do the projects, or what is the reason, if we have the funds, but we cannot get the projects done.

Dr. Thompson responded there are for various reasons. One reason is the right-of-way acquisition, so that holds up the process. When he thinks about Sunset, for example, the design work. Utility relocation was a difficult issue with that particular project. There is a laundry list of issues that will hold up a project from going to construction. He wants to reiterate that staff is not trying to hold up any process, in terms of slowing down productivity. We are working closely with the PDT. We want to make sure the funds are there to fund these projects.

Ms. McBride stated that is not an issue with her because she knows staff works hard to get things done, as well as the PDT. Her concern is, going back to Mr. Livingston, she remembers having a long debate over the \$250M, and we were told we could have these projects done. She wants to make sure that we are fiscally responsible, so she agrees with Mr. Gomeau, on that part, but she is not sure, with the other projects forthcoming, if we could run into those same issues that you are telling me, and that could slow the process, and there is no way to remove any of the issues so we could move forward with the projects. She just could not understand the lack of the ability to complete the projects.

Dr. Thompson stated, in his review of the project list, and talking with Mr. Beaty, he is comfortable with investing a little over \$100M for projects for next fiscal year. He does not think that Mr. Beaty will say he will be able to do \$250M worth of projects in one fiscal year.

Mr. Beaty stated the numbers Dr. Thompson has been providing is what the PDT provided; about \$100M - \$150M for the next couple fiscal years. He may be providing more information than Ms. McBride was asking. The Transportation Program, minus the buses, is about \$750M. Our current estimates, to build everything, is about \$900M, so somewhere there is about \$150M of work, based on today's dollars, that either cannot be done, or outside funding is needed. Going back to June, or so, the PDT provided you with 2 - 3 different scenarios of how you could maximize the amount of work. He does want to confirm the numbers in the 2012 referendum were not enough to keep up with inflation and construction moving forward. At some point, Council is going to have to make a decision, if we cannot bring in outside funds, some projects are not going to be able to be funded under this program.

Mr. Livingston moved, seconded by Mr. N. Jackson, to forward to Council without a recommendation.

In Favor: Malinowski, C. Jackson, N. Jackson, Livingston and McBride

The vote in favor was unanimous.

4. **Approval of Jushi's Letter Requesting Extension for the Shop Road Extension Project** – Dr. Thompson stated they have been approached by Economic Development and China Jushi regarding the addition of a separate employee and truck entrance. This was not thought of at the beginning, in terms of their design. The PDT and the contractors are out there building the Shop Road Extension, and wanted to bring this to the committee's attention. From staff's perspective, our position is, if we delay this project, the County should not be held responsible for one penny of any delays.

Mr. Livingston inquired if Dr. Thompson is referring to the letter in the agenda packet.

Dr. Thompson responded in the affirmative.

Mr. Livingston stated he thought they were requesting for us to hold up extending the road through where their plant is.

Dr. Thompson stated they want to delay the construction.

Mr. Livingston stated, for clarification, they want to delay us from extending the road through where their plant is.

Dr. Thompson responded in the affirmative.

Mr. Livingston inquired as to what costs Dr. Thompson was referring to.

Dr. Thompson stated, in the agenda packet on p. 6, Mr. Beaty provided information that one month of delay is going to cost about \$30,000 for 2 inspectors. He wants you to keep that in mind, as you make a decision about any delays.

Mr. Livingston inquired if that means you have to keep the inspectors on longer, and you have to pay to keep them on longer.

Mr. C. Jackson stated the Vice President of Operations from Jushi, who provided the letter, is here. He requested that he come down to the podium to speak regarding the matter.

Mr. Ray Wierzbowski stated, as far as the request from Jushi, that is our only access into the plant. We want to put a separate lane in for the truck access versus the employee cars, from a safety standpoint. Opening Shop Road, at this point, until we get that installed, will cause a safety issue of installing that turn lane, that was not thought of at the time. He stated they are requesting to delay this until March 31st to give them time to install. Jushi is paying for the turn lane and engineering. They have submitted their plans to SCDOT and are awaiting their approval. As far as the request for additional funding, he was not aware of that.

Mr. Livingston inquired as to when Shop Road is expected to open.

Dr. Thompson stated the end of January.

Mr. Livingston inquired as to the length of the delay.

Dr. Thompson stated it would be approximately a 2-month delay.

Mr. Livingston inquired as to why it would cost that much for the additional inspectors.

Mr. Beaty stated that is the cost for 2 additional inspectors, for 2 months.

Mr. Livingston stated, for clarification, if we were through with the road, what would we be inspecting.

Dr. Thompson stated we will not be through until it is fully inspected, and SCDOT receives the road. Until that time, it is still considered a County project.

Mr. Beaty stated to make sure the SCDOT accepts the road, and any new improvements, it must be inspected. The PDT plans on having 2 inspectors on the job through January. We cannot continue to provide inspectors indefinitely. At some point, we have to stop. If you ask us to have 2 more inspectors, for 2 more months, there is a cost.

Mr. Livingston inquired if there was any way to put up cones, barricades, or make it safe on one side. He stated he sees people work on roads all the time with cars going by.

Mr. Beaty stated he is not positive. He deferred to Mr. Wierzbowski.

Mr. Wierzbowski stated he is not sure if there is an option there, as far as cones.

Mr. Livingston stated he was just using that as an example. It could be anything. He stated he just sees people working on roads, and cars going by.

Mr. Wierzbowski stated, the concern from Jushi is, without the truck access, the turn lane for us...He stated they are the only ones on the road, at this point. Eventually, the industrial park will fill up, and they welcome neighbors. At this point, they are the only ones using this road, when it does open up, so that would limit and cut off our truck access, until they could get the turn lane in. He stated that is their concern with the delay.

Mr. Livingston stated, from the County's perspective, if the costs for the inspectors was covered somehow, would we be okay with holding it up.

Dr. Thompson responded in the affirmative.

Mr. N. Jackson stated, for clarification, Phase II is in design.

Mr. Beaty responded in the affirmative.

Mr. N. Jackson stated, for clarification, Phase I ends at Montgomery Road.

Mr. Beaty stated it ends at Longwood.

Mr. N. Jackson stated, for clarification, the way it is designed, and ends at Longwood, Shop Road Extension is a 5-lane, divided highway.

Mr. Beaty stated it is currently 4-lane, divided; 2 lanes in each direction with a grass median.

Mr. N. Jackson stated, his concern is, Jushi will be only plant for at least 6 months. The level of service for that road will remain "A" until it is opened. He could see if it was complete, with Phase II, then immediately you would have heavy traffic. Right now, with just that plant, and the few cars getting off Bluff Road, it does not make sense to have heavy traffic. He does not see why cones would not help. The amount is negligible. He stated it is a newly opened road, if cones are there, and one lane is open, he does not think it will affect any

traffic. He does not see any reason to stop it, or hold it up.

Mr. Malinowski stated what is being requested, as far as the actual development, is something that is outside of the scope of the Shop Road Project, correct?

Dr. Thompson responded in the affirmative.

Mr. Malinowski stated, if that is case, what is the possibility of going out and hiring inspectors outside of the PDT. Put it out for bid. See if you can get something with a lower cost.

Dr. Thompson stated it being outside of the scope of the project means it is outside of the scope of spending Penny funds.

Ms. McBride inquired if we are required, as a safety issue, to continue to have those inspectors, or is this something that you think is needed.

Mr. Beaty stated he would think the inspectors are needed full-time, while there is active construction going on. Again, the risk you would incur is, if in the SCDOT perspective, the County has entered into an intergovernmental agreement that says, "You will inspect it, appropriately", and it if there were no inspection, then the SCDOT could say, "We do not know what you did. We are not taking the road." To Mr. Malinowski's point, he is correct, the PDT does not have to do the inspection. If you could find another inspector(s) that would be your prerogative.

Ms. McBride stated the funding would be the issue there, unless Jushi is willing to pay for the inspectors.

Mr. Wierzbowski stated he is not prepared to answer that today. He would have to confer with his company.

Ms. McBride stated she thinks that is where we are now, in terms of who will pay for the inspectors. It is not the issue of whether or not to allow Jushi to do what they need to do.

Mr. Malinowski stated, for clarification, Jushi is paying for the additional part of the project.

Mr. Wierzbowski responded in the affirmative. He stated they do not have a finalized number, but the number is going to be close to \$300,000 for the turn lane. They feel that is an important safety factor.

Mr. Livingston inquired as to what staff's recommendation would be.

Mr. C. Jackson stated staff is recommending that we delay it, but that we not cover the costs for the delay.

Mr. N. Jackson stated, for clarification, Jushi wants the project delayed to build the turn lane. The reason they think it should be delayed is because the affect it may have on traffic using that road. From where he sees it, he does not see any traffic. The road will be open, but he does not see the use of the road to a level where it will affect any additional construction. In his years of experience, he cannot see that because there will not be that much traffic on that road until it is fully opened, and Phase II is complete.

Mr. Livingston stated, for clarification, Council will not meet again until December 11th. He inquired if we can move it forward to Council, and have Mr. Wierzbowski approach the company about the possibility of inspectors, and take action on December 11th. It seems to him having to pay the inspectors is the issue.

Mr. N. Jackson stated there are 2 issues. He inquired if it has to be delayed because of the use of the road.

Mr. Livingston stated he said that because staff had no objection with it being delayed. He was supporting that.

Mr. C. Jackson suggested Mr. N. Jackson make a motion not to delay the project and incorporate Mr. Livingston's comments that they investigate ways ameliorate the conditions, while they continue to work on the turning lane.

Mr. N. Jackson moved, seconded by Mr. Livingston, to not delay the project and to have staff, PDT and Jushi find ways either with the inspectors, or with cones, to move forward without affecting the road and safety issues.

Mr. Malinowski requested the motion be restated.

Mr. C. Jackson stated the motion was to not delay the project and have staff work with Jushi to figure out ways to work around the concerns they have regarding safety, as they continue to work on the turning lane, but that the County finish their work on the project at the end of January.

Mr. Malinowski inquired if that is something staff feels we can do.

Dr. Thompson stated, from a safety issue, staff is not comfortable with that. And, as Mr. Beaty, mentioned we will have to have inspectors on the premises until we turn the road over to SCDOT. If we continue with construction, Jushi is going to adversely impact the current project with them installing the turn lane. It is going to impact the grass median.

Mr. Malinowski stated that is what he was thinking. The County is going to be ready to turn this project over before Jushi's portion is completed. As Dr. Thompson stated, the County is not going to be responsible for any costs associated with the delay. It seems like there will delay because the County will be ready to turn it over. Is there any way the County can turn the road over, and Jushi get whatever permits they need and begin building on their own?

Dr. Thompson stated that is an option.

Mr. N. Jackson withdrew his motion. He stated there is not a lot of construction during this season. Construction may not start until March/April, so we have to consider that also.

Mr. Malinowski moved, seconded by Mr. Livingston, when Shop Road Extension is completely, and ready to be turned over, that is so done. Anything that needs to be constructed after that will be the responsibility of Jushi. Jushi can inquire with their company, and provide additional information at the December 11th Council meeting.

In Favor: Malinowski, C. Jackson, N. Jackson, Livingston and McBride

The vote in favor was unanimous.

5. **Approval of Letter Recommending Awarding Bid For Candlewood Neighborhood Improvements** – Mr. Livingston moved, seconded by Mr. Malinowski, to approve staff's recommendation.

In Favor: Malinowski, C. Jackson, N. Jackson, Livingston and McBride

The vote in favor was unanimous.

6. **Approval of Letter Recommending Awarding Bid for Pedestrian Improvement Package 2** – Mr. Malinowski moved, seconded by Mr. Livingston, to approve staff's recommendation.

Dr. Thompson stated the projects include Assembly Street and Calhoun Street Pedestrian Improvements – District 4, Assembly Street and Gervais Street Pedestrian Improvements – District 4 and 5, etc. There is a total of 17 sub-projects that fall under the category of Package 2.

In Favor: Malinowski, C. Jackson, N. Jackson, Livingston and McBride

The vote in favor was unanimous.

7. **ADJOURN** – The meeting adjourned at approximately 2:04 PM

EXECUTIVE SUMMARY

Date: January 3, 2019

To: Dr. John Thompson
Director of Transportation

From: David Beaty, PE
Program Manager

RE: Trenholm Acres/ New Castle NIP – Public Meeting Summary with Recommendations

The Trenholm Acres/ New Castle Neighborhood Improvement Project (Trenholm Acres/ New Castle NIP) is one of seven Neighborhood Improvement Projects included in the 2012 Referendum, with a total budgeted amount of \$5.39 million. The Richland County Transportation Program conducted one public meeting for the Trenholm Acres/ New Castle NIP, and completed conceptual studies. This Executive Summary will provide an overview of the public meeting and offer recommendations to advance the project.

November 15, 2018 Public Meeting

The Richland County Transportation Program held a public meeting for the Trenholm Acres/ New Castle NIP on Thursday, November 15, 2018 from 5:00 to 7:00 p.m. at the Trenholm Acres New Castle Community Center, located at 5819 Shakespeare Rd. The meeting was an informal, open house format with project displays and Richland County Transportation Program representatives present to answer questions. As people entered the meeting, staff provided a handout and a comment card, and encouraged the public to provide comments and rank the proposed improvements in the neighborhood plan, after they reviewed the displays and asked questions they may have. In addition to staff, 43 people attended the meeting.

The project displays provided an aerial overview map and typical sections of the proposed the neighborhood improvements included in Appendix A. The proposed improvements include sidewalks on Shakespeare Rd, Claudia Dr, Warner Dr, Westmore Dr, Sprott St, and Nancy Ave; and Streetscapes on Two Notch Rd, Fontaine Rd, and Parklane Rd. The major streetscapes include sidewalk repairs and planted medians.

During the comment period, staff received 37 comment cards and emails. The following lists the improvements in order of preference based on the numerical value that the public ranked projects with a lower score being a more desired project:

1. Shakespeare Road Sidewalk- 3.56
2. Claudia Drive Sidewalk- 3.94
3. Warner Drive Sidewalk- 4.69
4. Westmore Drive Sidewalk- 4.81
5. Humphrey Drive Sidewalk- 5.24
6. Two Notch Road Streetscape- 5.69
7. Fontaine Road Streetscape- 5.8
8. Sprott Street Sidewalk- 6.69
9. Nancy Avenue Sidewalk- 6.75
10. Parklane Streetscape- 6.8

Of the comments received in favor of sidewalks, 14 did not properly rank the projects, so their rankings were not incorporated into the overall rankings but were considered in the final recommendations. Three comments received concerned children's safety and the need for sidewalks to help keep children safe. Three comments asked for speed bumps on Claudia drive. Appendix B contains a summary of all public comments and preferences.

Recommendations

As a result of the comments received from the public meeting and coordination with project stakeholders, as well as safety considerations, project impacts, and available funding, the following projects are recommended for further design studies. Although the Two Notch and Fontaine Road Streetscapes were ranked marginally higher than the Sprott Street and Nancy Avenue Sidewalks, these two are not recommended for further study due to the minimal opportunity for landscaping the medians due to traffic/access issues, long-term maintenance by Richland County, and also due to the fact they each currently provide sidewalk accommodations. Additionally, conceptual cost estimates find these projects to total approximately to the project budget:

1. Shakespeare Road Sidewalk
2. Claudia Drive Sidewalk
3. Warner Drive Sidewalk
4. Westmore Drive Sidewalk
5. Humphrey Drive Sidewalk
6. Sprott Street Sidewalk
7. Nancy Avenue Sidewalk

Appendix A: November, 15, 2018 Public Meeting Information

Appendix B: Public Comments

TRENHOLM ACRES & NEWCASTLE

NEIGHBORHOOD IMPROVEMENTS
PUBLIC INFORMATION MEETING
NOVEMBER 15, 2018

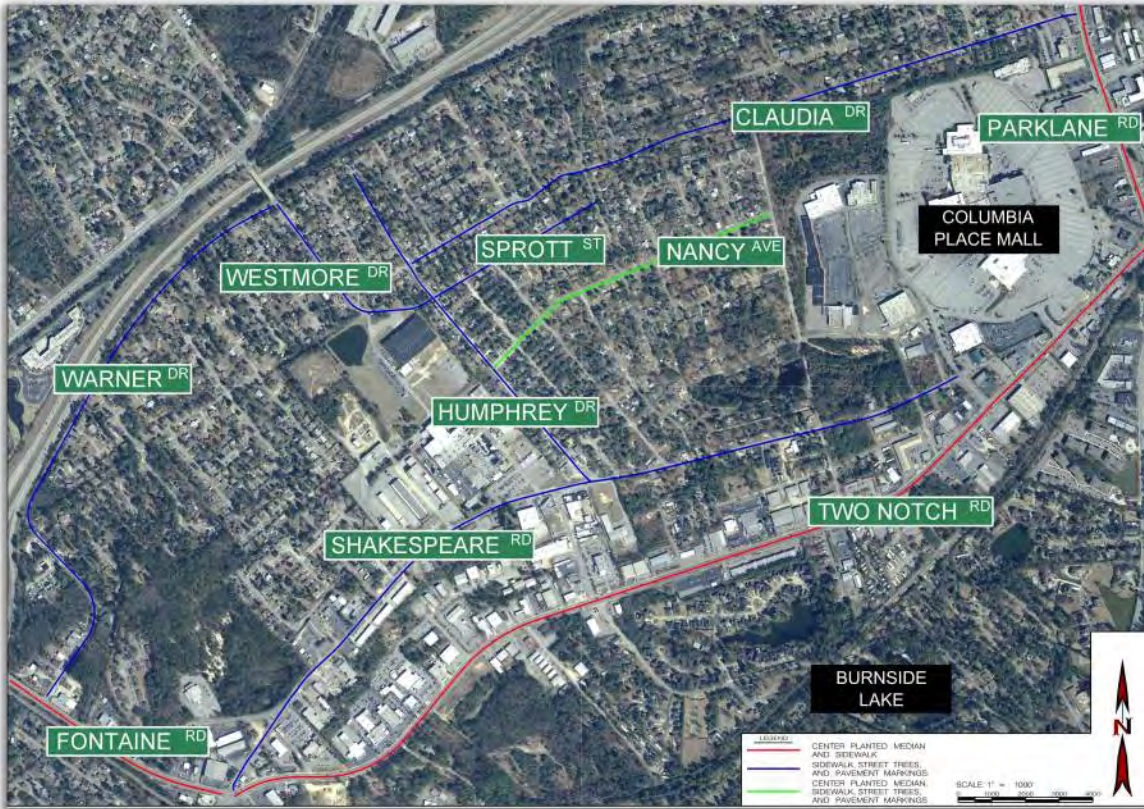


Mead & Hunt





**TRENHOLM ACRES & NEWCASTLE NEIGHBORHOOD IMPROVEMENTS
PUBLIC INFORMATION MEETING
NOVEMBER 15, 2018**



CONCEPT DESIGN

CLAUDIA DRIVE, HUMPHREY DRIVE, SHAKESPEARE ROAD,
SPROTT STREET, WARNER DRIVE, AND WESTMORE DRIVE



SCOPE: SIDEWALK, STREET TREES, AND PAVEMENT MARKINGS

CLAUDIA DRIVE



HUMPHREY DRIVE



SHAKESPEARE ROAD



SPROTT STREET



WARNER DRIVE

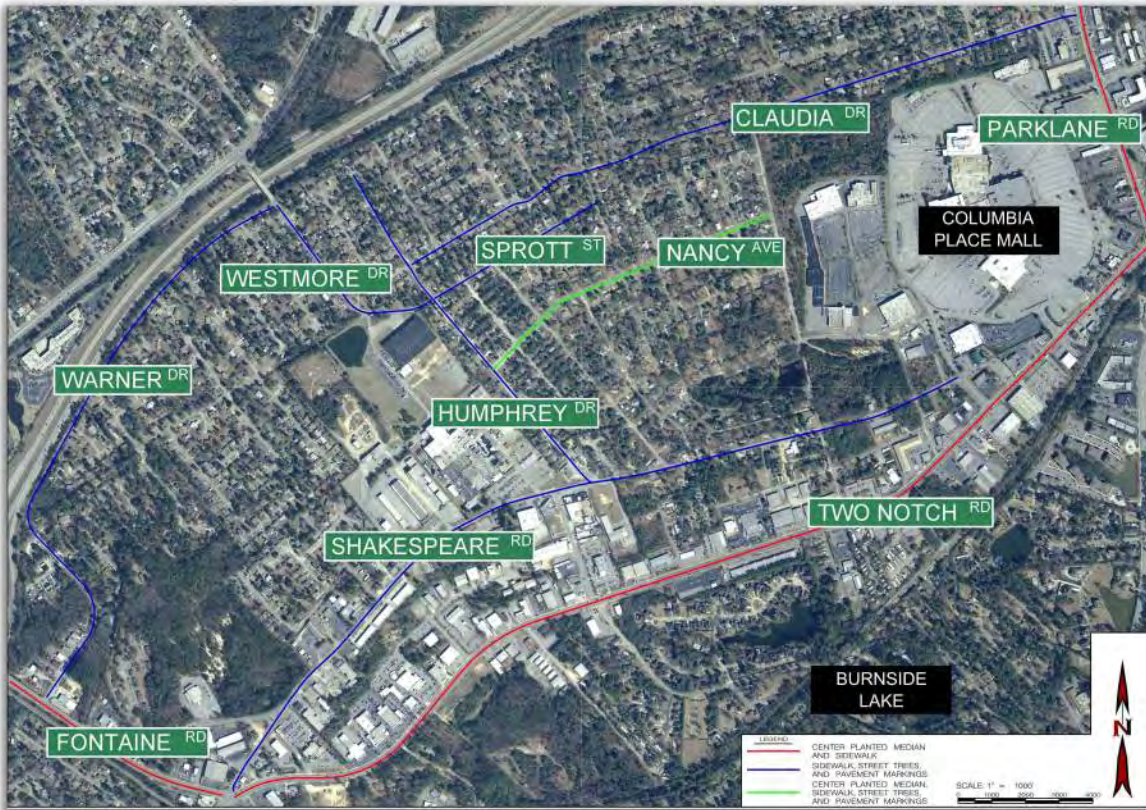


WESTMORE DRIVE





**TRENHOLM ACRES & NEWCASTLE NEIGHBORHOOD IMPROVEMENTS
PUBLIC INFORMATION MEETING
NOVEMBER 15, 2018**



FONTAINE ROAD



BEFORE



AFTER

TWO NOTCH ROAD



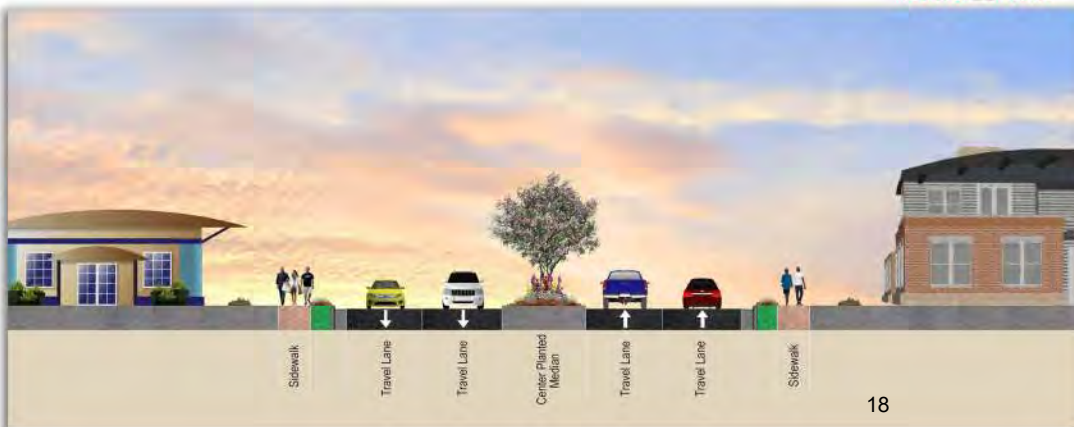
BEFORE



AFTER

CONCEPT DESIGN

FONTAINE ROAD, TWO NOTCH ROAD, AND PARKLANE ROAD



PARKLANE ROAD



BEFORE

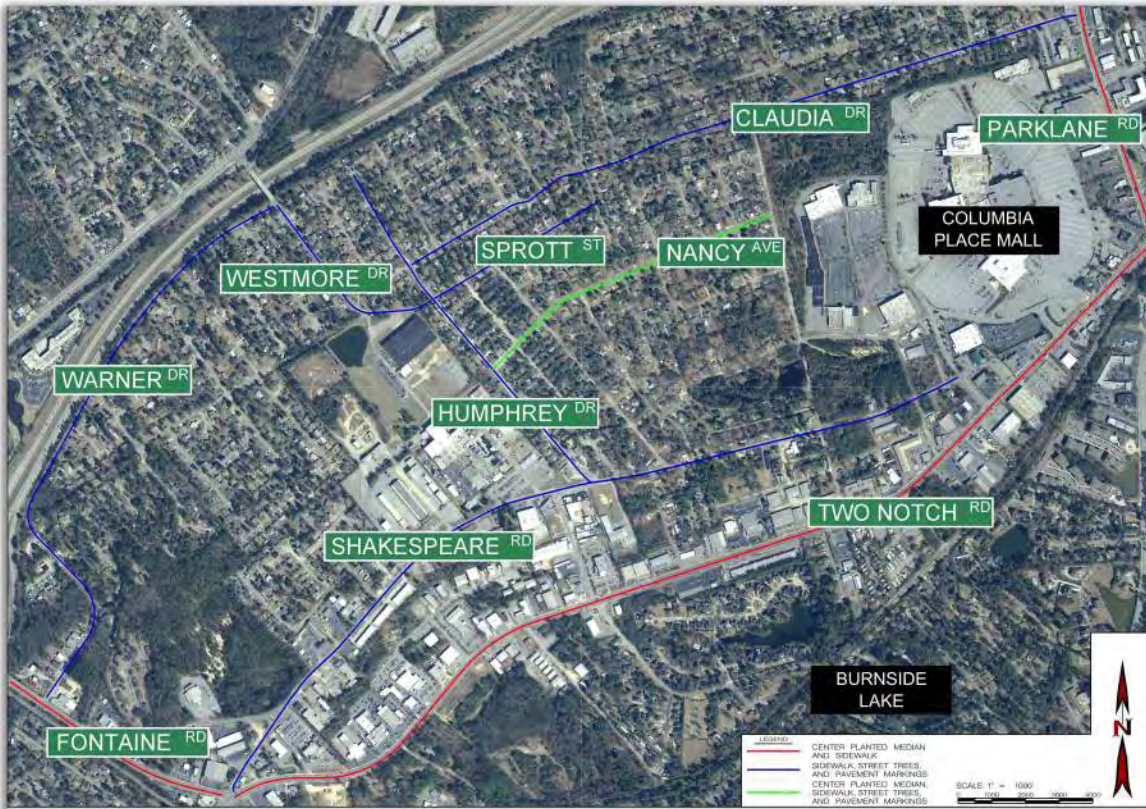


AFTER

SCOPE: CENTER PLANTED MEDIAN, ROADWAY SIGNAGE, AND SIDEWALK



TRENHOLM ACRES & NEWCASTLE NEIGHBORHOOD IMPROVEMENTS PUBLIC INFORMATION MEETING NOVEMBER 15, 2018



NANCY AVENUE



BEFORE



AFTER

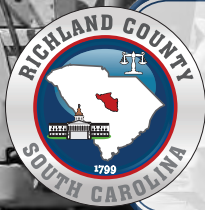
CONCEPT DESIGN

NANCY AVENUE



SCOPE:

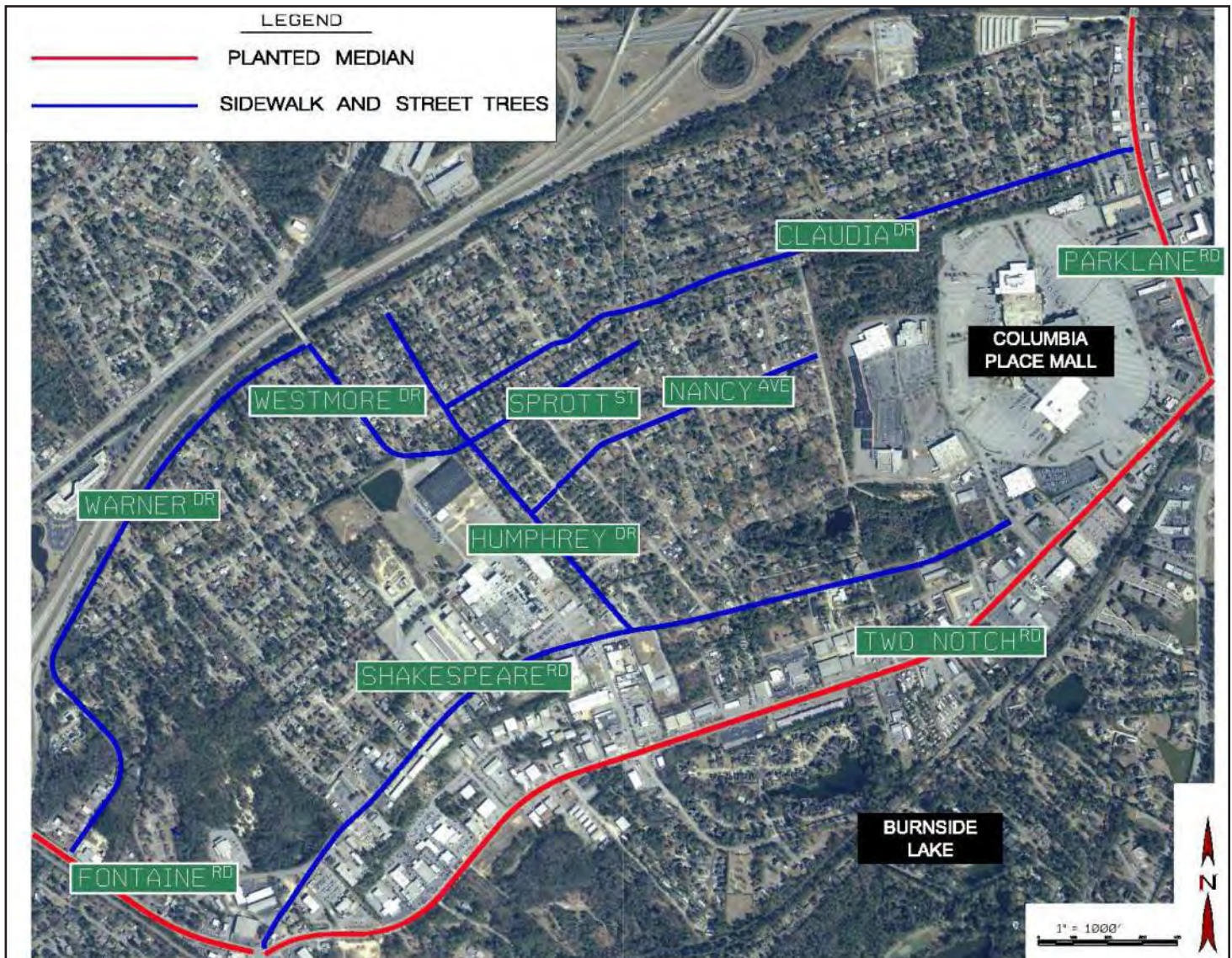
**CENTER PLANTED MEDIAN,
SIDEWALK, STREET TREES,
AND PAVEMENT MARKINGS**



TRANSPORTATION PROGRAM PUBLIC MEETING

TRENHOLM ACRES/ NEW CASTLE NEIGHBORHOOD IMPROVEMENT

Public Information Meeting – November 15, 2018



WELCOME

The purpose of the meeting is to gather input from the local community, concerned citizens, and project stakeholders on the proposed improvements in the Trenholm Acres/ New Castle Neighborhood area. You are encouraged to review the various displays and discuss your questions or concerns with any of the Richland Penny Program representatives.

PROJECT OVERVIEW

The Richland Penny Program is proposing neighborhood improvements to the Trenholm Acres/ New Castle Neighborhood area. The project includes landscaped medians and sidewalks with trees and road markings for improve safety. The proposed improvements will take place on Parkline Road, Two Notch Road, Fontaine Road, Shakespeare Road, Nancy Avenue, Warner Drive, Westmore Drive, Sprott Street, Claudia Drive and Humphrey Drive shown on the map above.



TRANSPORTATION PROGRAM PUBLIC MEETING

TRENHOLM ACRES/ NEW CASTLE NEIGHBORHOOD IMPROVEMENT

Public Information Meeting – Comment Card
November 15, 2018

Richland County Department of Transportation
2020 Hampton Street
PO Box 192
Columbia, SC 29201

How did you learn about the meeting?

Flyer Radio TV Newspaper Road Sign Word of Mouth Other _____

Please submit comments by November 30, 2018 in one of the following ways:

Drop this form in the comment card box before you leave the meeting.

Mail this card to Richland County Dept. of Transportation, 2020 Hampton St., PO Box 192, Columbia, SC 29201.

Email comments to transportationpenny@richlandcountysc.gov.

Call 844-RCPenny (844-727-3669) for more information about this project.

Appendix B

TRENHOLM ACRES NEW CASTLE NEIGHBORHOOD IMPROVEMENT PUBLIC MEETING COMMENTS 11/15/18

#	Comment										
1	<p>Please rank the projects 1-10, 1 being most desired and 10 being least desired:</p> <table style="width: 100%; border: none;"> <tr> <td style="width: 50%;"><u>10</u> Fontaine Road Streetscape</td> <td style="width: 50%;">_____ Nancy Avenue Sidewalk</td> </tr> <tr> <td><u>10</u> Two Notch Road Streetscape</td> <td>_____ Sprott Street Sidewalk</td> </tr> <tr> <td><u>10</u> Parklane Road Streetscape</td> <td><u>1</u> Claudia Drive Sidewalk</td> </tr> <tr> <td><u>1</u> Shakespeare Road Sidewalk</td> <td>_____ Westmore Drive Sidewalk</td> </tr> <tr> <td><u>1</u> 10 Humphrey Drive Sidewalk</td> <td>_____ Warner Drive Sidewalk</td> </tr> </table>	<u>10</u> Fontaine Road Streetscape	_____ Nancy Avenue Sidewalk	<u>10</u> Two Notch Road Streetscape	_____ Sprott Street Sidewalk	<u>10</u> Parklane Road Streetscape	<u>1</u> Claudia Drive Sidewalk	<u>1</u> Shakespeare Road Sidewalk	_____ Westmore Drive Sidewalk	<u>1</u> 10 Humphrey Drive Sidewalk	_____ Warner Drive Sidewalk
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<u>1</u> Humphrey Drive Sidewalk	<u>1</u> Warner Drive Sidewalk										

9	<p>I am a resident of the Trenholm Acres Extension Community and these are my concerns from the Public Information Meeting I attended on 11/15/18.</p> <p>Based on the information presented at the Meeting, your plans are set. However, this is my input for what its worth.</p> <ol style="list-style-type: none"> 1. Two Notch Road Streetscape 2. Claudia Drive Sidewalk 3. Shakespeare Road Sidewalk 4. Humphrey Drive Sidewalk 5. Westmore Drive Sidewalk <p><u>Additional Concerns</u></p> <p>I do not understand why none of the streets to the RIGHT of Claudia Drive are included in this Improvement. We are not exempt from the tax that has been imposed and I would like some answers. In the future, it would be good to get input from the taxpayers/community before (YOU) put the "Plan" in place; at least they feel as if they had input. Thank you.</p>		
10	<p>Hello,</p> <p>I hope all is well. I am a resident of this neighborhood and I reside on Judy St. I strongly agree sidewalks should be added to the proposed streets AS WELL as Judy St. Humphrey Drive should be repaved with a quickness. There are several large vehicle damaging potholes. Westmore Drive has a huge litter issue that should be addressed along with adding a sidewalk.</p> <p>I strongly opposed the Fontaine, Parklane and Two Notch streetscape. For obvious reasons, please refer to Nancy Ave streetscape and its disrepair. I'm certain when the streetscape was completed it was beautiful. However, it takes regular maintenance to ensure the nice and neat appearance which SCDOT/ County/City does not provide. I have expressed my concern several times regarding the overgrown grass, litter and the unpruned trees that impede motorists' visibility to no avail. The Nancy Ave streetscape remains unkept and unsightly. It doesn't add any value to the neighborhood. It lessens the value as does the continuous litter, illegal dumping and garbage issues in this neighborhood. Reference N. Lake Marian Circle behind Value City between Roof St. and Columbia Mall Rd.</p> <p>Shakespeare Rd is also unkept. It is frequently littered, the grass and the trees are overgrown. There isn't any scheduled litter pick up or streetcleaning. Residents must request litter pickup even though the SCDOT/County/City is aware of the highly littered area. It takes 30-45 days for the request to be processed. However, please note, recently the trees were trimmed from the road.</p> <p>Two Notch Rd is always littered and public domains are unkept (overgrown grass/trees) from Home Depot to Beltline. Please note Parklane/Decker and Two Notch by overpass. Unoccupied houses/buildings are in ruins due to absentee owners not maintaining their properties. This is should be addressed. Ironically, the further you travel up Two Notch Rd, towards Spring Valley, the cleaner and manicured it becomes. The streetscape of these areas in long term will not be of any value due to the required maintenance. Improving the maintenance of the neighborhood would improve the appearance and value of the neighborhood.</p>		
11	<p>Please rank the projects 1-10, 1 being most desired and 10 being least desired:</p> <table border="0" style="width: 100%;"> <tr> <td style="width: 50%; vertical-align: top;"> Fontaine Road Streetscape Two Notch Road Streetscape Parklane Road Streetscape Shakespeare Road Sidewalk <u>2</u> Humphrey Drive Sidewalk </td> <td style="width: 50%; vertical-align: top;"> <u>2</u> Nancy Avenue Sidewalk <u>2</u> Sprott Street Sidewalk <u>2</u> Claudia Drive Sidewalk _____ Westmore Drive Sidewalk <u>10</u> Warner Drive Sidewalk </td> </tr> </table>	Fontaine Road Streetscape Two Notch Road Streetscape Parklane Road Streetscape Shakespeare Road Sidewalk <u>2</u> Humphrey Drive Sidewalk	<u>2</u> Nancy Avenue Sidewalk <u>2</u> Sprott Street Sidewalk <u>2</u> Claudia Drive Sidewalk _____ Westmore Drive Sidewalk <u>10</u> Warner Drive Sidewalk
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	<p>A lot of streets are missing... Hazel St.; Parkingson Dr; Pinedale; Parkview. Need speed bumps- sidewalks- roads fixed. I would rather see within our own neighborhood, than out on Fontaine, Two Notch, Parklane, or Shakespeare.</p>										
<p>12</p>	<p>Please rank the projects 1-10, 1 being most desired and 10 being least desired:</p> <table border="0"> <tr> <td><input type="checkbox"/> Fontaine Road Streetscape</td> <td><input checked="" type="checkbox"/> Nancy Avenue Sidewalk</td> </tr> <tr> <td><input type="checkbox"/> Two Notch Road Streetscape</td> <td><input type="checkbox"/> Sprott Street Sidewalk</td> </tr> <tr> <td><input checked="" type="checkbox"/> Parklane Road Streetscape</td> <td><input type="checkbox"/> Claudia Drive Sidewalk</td> </tr> <tr> <td><input type="checkbox"/> Shakespeare Road Sidewalk</td> <td><input type="checkbox"/> Westmore Drive Sidewalk</td> </tr> <tr> <td><input checked="" type="checkbox"/> Humphrey Drive Sidewalk</td> <td><input type="checkbox"/> Warner Drive Sidewalk</td> </tr> </table> <p>The median on Nancy need to be remodeled and maintained. Also it would be very helpful to have slow children playing also bus stops indicated within the neighborhood.</p>	<input type="checkbox"/> Fontaine Road Streetscape	<input checked="" type="checkbox"/> Nancy Avenue Sidewalk	<input type="checkbox"/> Two Notch Road Streetscape	<input type="checkbox"/> Sprott Street Sidewalk	<input checked="" type="checkbox"/> Parklane Road Streetscape	<input type="checkbox"/> Claudia Drive Sidewalk	<input type="checkbox"/> Shakespeare Road Sidewalk	<input type="checkbox"/> Westmore Drive Sidewalk	<input checked="" type="checkbox"/> Humphrey Drive Sidewalk	<input type="checkbox"/> Warner Drive Sidewalk
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<p>13</p>	<p>Please rank the projects 1-10, 1 being most desired and 10 being least desired:</p> <table border="0"> <tr> <td><input type="checkbox"/> Fontaine Road Streetscape</td> <td><input checked="" type="checkbox"/> Nancy Avenue Sidewalk</td> </tr> <tr> <td><input type="checkbox"/> Two Notch Road Streetscape</td> <td><input checked="" type="checkbox"/> Sprott Street Sidewalk</td> </tr> <tr> <td><input type="checkbox"/> Parklane Road Streetscape</td> <td><input checked="" type="checkbox"/> Claudia Drive Sidewalk</td> </tr> <tr> <td><input checked="" type="checkbox"/> Shakespeare Road Sidewalk</td> <td><input checked="" type="checkbox"/> Westmore Drive Sidewalk</td> </tr> <tr> <td><input checked="" type="checkbox"/> Humphrey Drive Sidewalk</td> <td><input checked="" type="checkbox"/> Warner Drive Sidewalk</td> </tr> </table> <p>I live at the end of Humphrey Drive ALL the water comes in my yard from Parkingson Dr. and Humphrey Drive. I have complained about this for 10 years. They said they were going to put a drain in my area, then they cancelled it. They paved the road about two weeks ago and that has made it worse. My yard fills up with water and it flood the inside of my house. Water is across the whole front of my house. NEED A DRAINAGE SYSTEM IN THE AREAD IN FRONT OF MY HOUSE. THANK YOU.</p>	<input type="checkbox"/> Fontaine Road Streetscape	<input checked="" type="checkbox"/> Nancy Avenue Sidewalk	<input type="checkbox"/> Two Notch Road Streetscape	<input checked="" type="checkbox"/> Sprott Street Sidewalk	<input type="checkbox"/> Parklane Road Streetscape	<input checked="" type="checkbox"/> Claudia Drive Sidewalk	<input checked="" type="checkbox"/> Shakespeare Road Sidewalk	<input checked="" type="checkbox"/> Westmore Drive Sidewalk	<input checked="" type="checkbox"/> Humphrey Drive Sidewalk	<input checked="" type="checkbox"/> Warner Drive Sidewalk
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16 Please rank the projects 1-10, 1 being most desired and 10 being least desired:

1 Fontaine Road Streetscape	_____ Nancy Avenue Sidewalk
1 Two Notch Road Streetscape	_____ Sprott Street Sidewalk
1 Parklane Road Streetscape	<u>✓</u> Claudia Drive Sidewalk
<u>✓</u> Shakespeare Road Sidewalk	_____ Westmore Drive Sidewalk
<u>✓</u> Humphrey Drive Sidewalk	_____ Warner Drive Sidewalk

Shakespeare Road & Humphrey Drive for sure need sidewalk. All the rest of these projects should remain the same with no improvements.

17 Please rank the projects 1-10, 1 being most desired and 10 being least desired:

<u>10</u> Fontaine Road Streetscape	<u>5</u> Nancy Avenue Sidewalk
<u>10</u> Two Notch Road Streetscape	<u>2</u> Sprott Street Sidewalk
<u>10</u> Parklane Road Streetscape	<u>1</u> Claudia Drive Sidewalk
<u>1</u> Shakespeare Road Sidewalk	<u>5</u> Westmore Drive Sidewalk
<u>1</u> Humphrey Drive Sidewalk	<u>1</u> Warner Drive Sidewalk

Pinedale Dr is in Trenholm Acres, which is not on this plan. The only street in Trenholm Acres that on this plan is Claudia Dr. I am only concerned about the streets in Trenholm Acres. I met with Council Jeter when they were planning this Penny Taxes. I told him I would vote for this Penny Tax if they would put sidewalks in Trenholm Acres. I disagree this plan. I was president of Trenholm Acres for years. I don't remember even attending a meeting on this plan.

18 Please rank the projects 1-10, 1 being most desired and 10 being least desired:

<u>10</u> Fontaine Road Streetscape	<u>4</u> Nancy Avenue Sidewalk
<u>10</u> Two Notch Road Streetscape	<u>3</u> Sprott Street Sidewalk
<u>4</u> Parklane Road Streetscape	<u>2</u> Claudia Drive Sidewalk
_____ Shakespeare Road Sidewalk	_____ Westmore Drive Sidewalk
_____ Humphrey Drive Sidewalk	_____ Warner Drive Sidewalk

I am presently president of Trenholm Acres neighborhood. I am appalled that Pinedale Dr, Oakmont, Parkington Dr, Parkview Dr, Arrowood Dr, are not listed. We have heard promises after promises for over 40 years; promised sidewalks, paved road; regulation of Commercial businesses in resident neighborhood with no evidence of your promises. We have been presented presentations before with no follow-up or follow through. It is time to stop making empty promises and begin serving the people who services most!!! Yes, I am upset, but I am pre-active. I believe that Richland County will hear my cry and be moved to address the needs of all the people of Trenholm Acres. Than you for your consideration.

<p>19</p>	<p>Please rank the projects 1-10, 1 being most desired and 10 being least desired:</p> <table border="0"> <tr> <td><u> </u> Fontaine Road Streetscape</td> <td><u> 4 </u> Nancy Avenue Sidewalk - Sidewalks</td> </tr> <tr> <td><u> </u> Two Notch Road Streetscape</td> <td><u> 1 </u> Sprott Street Sidewalk</td> </tr> <tr> <td><u> </u> Parklane Road Streetscape</td> <td><u> 2 </u> Claudia Drive Sidewalk</td> </tr> <tr> <td><u> </u> Shakespeare Road Sidewalk</td> <td><u> </u> Westmore Drive Sidewalk</td> </tr> <tr> <td><u> 3 </u> Humphrey Drive Sidewalk</td> <td><u> 10 </u> Warner Drive Sidewalk</td> </tr> </table> <p>Hazel, Parkingson, Pinedale, Parkview, Nancy- sidewalk on both sides. I do not care anything about Fontaine, Two Notch, Parklane, Shakespeare, street. Done it does not affect our neighborhood. Please use the money to help the neighborhood.</p>	<u> </u> Fontaine Road Streetscape	<u> 4 </u> Nancy Avenue Sidewalk - Sidewalks	<u> </u> Two Notch Road Streetscape	<u> 1 </u> Sprott Street Sidewalk	<u> </u> Parklane Road Streetscape	<u> 2 </u> Claudia Drive Sidewalk	<u> </u> Shakespeare Road Sidewalk	<u> </u> Westmore Drive Sidewalk	<u> 3 </u> Humphrey Drive Sidewalk	<u> 10 </u> Warner Drive Sidewalk
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<u> 9 </u> Humphrey Drive Sidewalk	<u> 3 </u> Warner Drive Sidewalk										

23 Please rank the projects 1-10, 1 being most desired and 10 being least desired:

<u>8</u> Fontaine Road Streetscape	<u>4</u> Nancy Avenue Sidewalk
<u>9</u> Two Notch Road Streetscape	<u>5</u> Sprott Street Sidewalk
<u>10</u> Parklane Road Streetscape	<u>3</u> Claudia Drive Sidewalk
<u>1</u> Shakespeare Road Sidewalk	<u>6</u> Westmore Drive Sidewalk
<u>2</u> Humphrey Drive Sidewalk	<u>7</u> Warner Drive Sidewalk

Need to repave Humphrey Drive.

24 Please rank the projects 1-10, 1 being most desired and 10 being least desired:

<u>3</u> Fontaine Road Streetscape	<u>10</u> Nancy Avenue Sidewalk
<u>4</u> Two Notch Road Streetscape	<u>9</u> Sprott Street Sidewalk
<u>5</u> Parklane Road Streetscape	<u>6</u> Claudia Drive Sidewalk
<u>2</u> Shakespeare Road Sidewalk	<u>7</u> Westmore Drive Sidewalk
<u>8</u> Humphrey Drive Sidewalk	<u>1</u> Warner Drive Sidewalk

Warner Drive runs from Westmore to Fontaine. It's the frontage road to the New Castle community. Presently, the road is heavily littered. With the improvement w/sidewalk and trees and the present litter problem, it could become an even bigger eyesore. Whose responsibility would it be to keep it clean? If Warner Dr is a state road, will the County still be responsible for maintenance?

25 Please rank the projects 1-10, 1 being most desired and 10 being least desired:

<u>9</u> Fontaine Road Streetscape	<u>10</u> Nancy Avenue Sidewalk
<u>1</u> Two Notch Road Streetscape	<u>4</u> Sprott Street Sidewalk
<u>2</u> Parklane Road Streetscape	<u>6</u> Claudia Drive Sidewalk
<u>7</u> Shakespeare Road Sidewalk	<u>5</u> Westmore Drive Sidewalk
<u>8</u> Humphrey Drive Sidewalk	<u>3</u> Warner Drive Sidewalk

Two Notch Road and Parklane Road are too busy and congested for more trees. Just repair these two major roads and keep them cleaned. Re-do the other above listed streets, etc.

26 Please rank the projects 1-10, 1 being most desired and 10 being least desired:

<u>9</u> Fontaine Road Streetscape	<u>6</u> Nancy Avenue Sidewalk
<u>8</u> Two Notch Road Streetscape	<u>7</u> Sprott Street Sidewalk
<u>5</u> Parklane Road Streetscape	<u>4</u> Claudia Drive Sidewalk
<u>3</u> Shakespeare Road Sidewalk	<u>2</u> Westmore Drive Sidewalk
<u>1</u> Humphrey Drive Sidewalk	<u>10</u> Warner Drive Sidewalk

1= a lot of foot traffic/ vehicles; 3= main entry to area; 9= main entry to area; 2= main entry to area; 4= main entry to area

27

Please rank the projects 1-10, 1 being most desired and 10 being least desired:

- | | |
|-------------------------------------|----------------------------------|
| <u>10</u> Fontaine Road Streetscape | <u>5</u> Nancy Avenue Sidewalk |
| <u>8</u> Two Notch Road Streetscape | <u>16</u> Sprott Street Sidewalk |
| <u>9</u> Parklane Road Streetscape | <u>1</u> Claudia Drive Sidewalk |
| <u>2</u> Shakespeare Road Sidewalk | <u>3</u> Westmore Drive Sidewalk |
| <u>4</u> Humphrey Drive Sidewalk | <u>7</u> Warner Drive Sidewalk |

I desperately need upgrade with sidewalk. Our kids have to walk in the street or someone else's yard. The edge of the road is uneven so that makes it dangerous. We the residents of Trenholm Acres pay our fair of taxes too and deserve better representation. We love to feel safe in our community, and have pride and keep our community clean and beautiful.

28

Please rank the projects 1-10, 1 being most desired and 10 being least desired:

- | | |
|-------------------------------------|----------------------------------|
| <u>2</u> Fontaine Road Streetscape | <u>7</u> Nancy Avenue Sidewalk |
| <u>3</u> Two Notch Road Streetscape | <u>8</u> Sprott Street Sidewalk |
| <u>10</u> Parklane Road Streetscape | <u>5</u> Claudia Drive Sidewalk |
| <u>4</u> Shakespeare Road Sidewalk | <u>9</u> Westmore Drive Sidewalk |
| <u>6</u> Humphrey Drive Sidewalk | <u>1</u> Warner Drive Sidewalk |

All of these sites are a priority because we don't have a nearby park. We don't have facility to accommodate residents in various necessary ways. One of our main concerns is health. We need safe sidewalks for children during school days and moving about on the weekends. Our elderly don't have sidewalks that are safe so that they can exercise safely. Once the sidewalks are here then landscaping can be done. Who wants to walk in a neighborhood that is not beautiful or safe? Also New Castle needs more lighting. It is hard to drive at night without almost hitting a car or other hard to see vehicles. Again, all our needs need to be considered to make this a safe and a neighborhood for health-conscious residents.

29

Please rank the projects 1-10, 1 being most desired and 10 being least desired:

- | | |
|-------------------------------------|----------------------------------|
| <u>5</u> Fontaine Road Streetscape | <u>1</u> Nancy Avenue Sidewalk |
| <u>6</u> Two Notch Road Streetscape | <u>2</u> Sprott Street Sidewalk |
| <u>9</u> Parklane Road Streetscape | <u>3</u> Claudia Drive Sidewalk |
| <u>7</u> Shakespeare Road Sidewalk | <u>4</u> Westmore Drive Sidewalk |
| <u>8</u> Humphrey Drive Sidewalk | <u>10</u> Warner Drive Sidewalk |

Great idea to enhance the surrounding areas. I do appreciate the Penny Tax revenue. I just have a rental property the area, but live somewhere else.

<p>30</p>	<p>Please rank the projects 1-10, 1 being most desired and 10 being least desired:</p> <table border="0"> <tr> <td><u>3</u> Fontaine Road Streetscape</td> <td><u>10</u> Nancy Avenue Sidewalk</td> </tr> <tr> <td><u>4</u> Two Notch Road Streetscape</td> <td><u>9</u> Sprott Street Sidewalk</td> </tr> <tr> <td><u>5</u> Parklane Road Streetscape</td> <td><u>8</u> Claudia Drive Sidewalk</td> </tr> <tr> <td><u>2</u> Shakespeare Road Sidewalk</td> <td><u>7</u> Westmore Drive Sidewalk</td> </tr> <tr> <td><u>6</u> Humphrey Drive Sidewalk</td> <td><u>1</u> Warner Drive Sidewalk</td> </tr> </table> <p>New Castle subdivision need sidewalks in the community. Seniors and walkers need them within the community. For example: New Castle Dr, Weldwood Ct, Coolstream. Much safer to walk within the immediate community.</p>	<u>3</u> Fontaine Road Streetscape	<u>10</u> Nancy Avenue Sidewalk	<u>4</u> Two Notch Road Streetscape	<u>9</u> Sprott Street Sidewalk	<u>5</u> Parklane Road Streetscape	<u>8</u> Claudia Drive Sidewalk	<u>2</u> Shakespeare Road Sidewalk	<u>7</u> Westmore Drive Sidewalk	<u>6</u> Humphrey Drive Sidewalk	<u>1</u> Warner Drive Sidewalk
<u>3</u> Fontaine Road Streetscape	<u>10</u> Nancy Avenue Sidewalk										
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<u>2</u> Shakespeare Road Sidewalk	<u>7</u> Westmore Drive Sidewalk										
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<p>31</p>	<p>Please rank the projects 1-10, 1 being most desired and 10 being least desired:</p> <table border="0"> <tr> <td><u>9</u> Fontaine Road Streetscape</td> <td><u>8</u> Nancy Avenue Sidewalk</td> </tr> <tr> <td><u>10</u> Two Notch Road Streetscape</td> <td><u>7</u> Sprott Street Sidewalk</td> </tr> <tr> <td><u>3</u> Parklane Road Streetscape</td> <td><u>1</u> Claudia Drive Sidewalk</td> </tr> <tr> <td><u>2</u> Shakespeare Road Sidewalk</td> <td><u>5</u> Westmore Drive Sidewalk</td> </tr> <tr> <td><u>4</u> Humphrey Drive Sidewalk</td> <td><u>6</u> Warner Drive Sidewalk</td> </tr> </table> <p>More needed within the neighborhoods! As a resident of Pinedale in Trenholm Acres, I have interest in a sidewalk for my street. There are children and adults who walk on Pinedale regularly and at least three school buses pick-up on Pinedale most every year I've lived here 28 years. I really think there needs to be much more attention and money spent to improve the liveability within the neighborhood where people walk, play and commune. Less is needed on Two Notch and Fontaine roads that people travel by car and or use as thoroughfare. Put sidewalks inside the residential areas.</p>	<u>9</u> Fontaine Road Streetscape	<u>8</u> Nancy Avenue Sidewalk	<u>10</u> Two Notch Road Streetscape	<u>7</u> Sprott Street Sidewalk	<u>3</u> Parklane Road Streetscape	<u>1</u> Claudia Drive Sidewalk	<u>2</u> Shakespeare Road Sidewalk	<u>5</u> Westmore Drive Sidewalk	<u>4</u> Humphrey Drive Sidewalk	<u>6</u> Warner Drive Sidewalk
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<p>32</p>	<p>Please rank the projects 1-10, 1 being most desired and 10 being least desired:</p> <table border="0"> <tr> <td><u>10</u> Fontaine Road Streetscape</td> <td><u>3</u> Nancy Avenue Sidewalk</td> </tr> <tr> <td><u>9</u> Two Notch Road Streetscape</td> <td><u>5</u> Sprott Street Sidewalk</td> </tr> <tr> <td><u>8</u> Parklane Road Streetscape</td> <td><u>2</u> Claudia Drive Sidewalk</td> </tr> <tr> <td><u>7</u> Shakespeare Road Sidewalk</td> <td><u>4</u> Westmore Drive Sidewalk</td> </tr> <tr> <td><u>1</u> Humphrey Drive Sidewalk</td> <td><u>6</u> Warner Drive Sidewalk</td> </tr> </table> <p>I'm more interested in areas within the community. We have many persons who walk in community/streets. Would rather see money spent for more sidewalks over streetscapes Two Notch and Parklane since funding limited. Would like to see improvement to Nancy median.</p>	<u>10</u> Fontaine Road Streetscape	<u>3</u> Nancy Avenue Sidewalk	<u>9</u> Two Notch Road Streetscape	<u>5</u> Sprott Street Sidewalk	<u>8</u> Parklane Road Streetscape	<u>2</u> Claudia Drive Sidewalk	<u>7</u> Shakespeare Road Sidewalk	<u>4</u> Westmore Drive Sidewalk	<u>1</u> Humphrey Drive Sidewalk	<u>6</u> Warner Drive Sidewalk
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<u>1</u> Humphrey Drive Sidewalk	<u>6</u> Warner Drive Sidewalk										
<p>33</p>	<p>It would make more sense if the plan would continue to include the streets where some of the street that connect with the mall property and then go up to the new apartment community that is beginning and the side connecting road that is that go down to Two Notch. This might be included later but the road between the property and where we are now should all be included. This might get some of the business and homeowners to do something about the appearance they should contribute to the people who live in the community. This must cause some businesses to consider</p>										

	making themselves available to the public. We should be careful to make sure all the variations plans for areas are all inclusive even when it extends the two and the cost price to completion.										
34	Agree if the legends identified on the charts work to the done. In addition- sidewalks on both sides of the street pavement and sidewalks place within the neighborhood so that walking on the sidewalks are possible. Warner drive curve is very unsafe and needs sidewalk. Street needs to be structured so that cars can be seen in both directions. Very unsafe now. Please put sidewalks on the streets in the neighborhood. Also pave these streets.										
35	<p>Please rank the projects 1-10, 1 being most desired and 10 being least desired:</p> <table border="0"> <tr> <td><u>2</u> Fontaine Road Streetscape</td> <td><u>9</u> Nancy Avenue Sidewalk</td> </tr> <tr> <td><u>10</u> Two Notch Road Streetscape</td> <td><u>8</u> Spratt Street Sidewalk</td> </tr> <tr> <td><u>7</u> Parklane Road Streetscape</td> <td><u>5</u> Claudia Drive Sidewalk</td> </tr> <tr> <td><u>3</u> Shakespeare Road Sidewalk</td> <td><u>4</u> Westmore Drive Sidewalk</td> </tr> <tr> <td><u>6</u> Humphrey Drive Sidewalk</td> <td><u>1</u> Warner Drive Sidewalk</td> </tr> </table>	<u>2</u> Fontaine Road Streetscape	<u>9</u> Nancy Avenue Sidewalk	<u>10</u> Two Notch Road Streetscape	<u>8</u> Spratt Street Sidewalk	<u>7</u> Parklane Road Streetscape	<u>5</u> Claudia Drive Sidewalk	<u>3</u> Shakespeare Road Sidewalk	<u>4</u> Westmore Drive Sidewalk	<u>6</u> Humphrey Drive Sidewalk	<u>1</u> Warner Drive Sidewalk
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TRANSPORTATION PROGRAM

EXECUTIVE SUMMARY

Date: 2/12/19

To: John Thompson, Ph.D., MBA, CPM
Director of Transportation

From: David Beaty, PE
Program Manager

RE: Shop Road Extension Phase 2 - Concept Report and Public Meeting Summary with Recommendations

Introduction

The Shop Road Extension project is a Special Project included in the 2012 Referendum, with an allocated budget of \$71.8 million. The project extends Shop Road from Pineview Road to Garners Ferry Road. Phase 1 of the project, which extends from Pineview Road to Longwood Road, is currently under construction and nearing completion. It is estimated that \$41.3 million will remain to construct Phase 2 which will complete the extension to Garners Ferry Road. The purpose of this document is to summarize the conceptual studies and public input to date and provide recommendations to advance the project.

Concept Report

A Concept Report was prepared for the Shop Road Extension Phase 2 project which describes the existing project area conditions, proposed roadway typical section, four alternative roadway alignments, and the alternatives impact analysis. The report includes cost estimates and details impacts (environmental, rights-of-way (ROW), utility, etc.) for the alternatives. The report also considers other project variables such as at-grade versus grade-separated railroad crossings and the construction of a four lane versus two lane roadway. Refer to Exhibit A to view the Concept Report. Refer to Exhibit B to view the typical section and alignment alternatives.

December 6, 2018 Public Meeting

The Richland County Transportation Program held a Public Meeting for the Shop Road Extension Phase 2 project on Thursday, December 6th, 2018 from 5:00 to 7:00 p.m. at Bluff Road Park, 148 Carswell Drive in Columbia, SC. The meeting was conducted with an informal, open house format where individuals were able to review project displays of the proposed alignment alternatives and typical section and discuss questions with

Richland County Transportation Program representatives. As people entered the meeting, staff provided a project handout and comment card and encouraged the public to provide comments on the proposed alignment alternatives and typical section. Refer to Exhibit B for the public meeting material, including proposed alignment alternatives and typical section.

Thirty people attended the public meeting and a total of 12 written comments were received from the meeting and the following two week comment period. The individuals commented on the proposed alignment alternatives, specific impacts to the residents, project costs, traffic concerns, accessibility, the ROW acquisition process, and bicycle accommodations. The existing undeveloped characteristics of the project area were identified by residents' comments as important and thus dictated most of their preferences for the alternatives. Alternative 2 was strongly and consistently disapproved by residents along Lykesland Trail in order to preserve the rural road. One of the residents also disapproved of Alternative 4 due to noise potential. Three of the residents supported Alternative 3 to minimize impacts to their community and one also supported Alternative 1. Montgomery Lane residents gave preference to Alternate 1 and were not in favor of Alternative 3 due to the proximity of the proposed roadway. The public meeting summary is attached as Exhibit C and has further details on the input collected from the residents.

Prior to advancing the project into the ROW acquisition stage, the Richland County Transportation Program will hold another public meeting. This will allow the residents to view the selected alternative alignment, the proposed side road tie-ins and discuss other specific project concerns.

Recommendations

Based on the comments received at the public meeting as well as consideration of project impacts and costs, Alternative 4 with a two-lane roadway section is recommended for the Shop Road Extension Phase 2 project. Alternative 4 is financially feasible based on the expected project budget and has the fewest wetland and flood zone impacts. In addition, Alternative 4 has the second fewest stream impacts and requires the least area of ROW obtains. Only one comment was received in opposition to Alternative 4 due to noise concern. Although Alternative 2 and Alternative 4 are the closest to the subject property, the Alternative 4 alignment would be greater than 500 feet from the edge of the property.

The recommendation for Alternative 4 includes securing a 200-ft total width ROW and two bridge crossings. The proposed 200-ft ROW width is recommended to accommodate a future four-lane roadway. The two bridge crossings are over Mill Creek and over Norfolk Southern railroad. At this conceptual stage, a bridge is not proposed for the CSX Transportation railroad crossing due to low volume train traffic.

Exhibit A: Concept Report

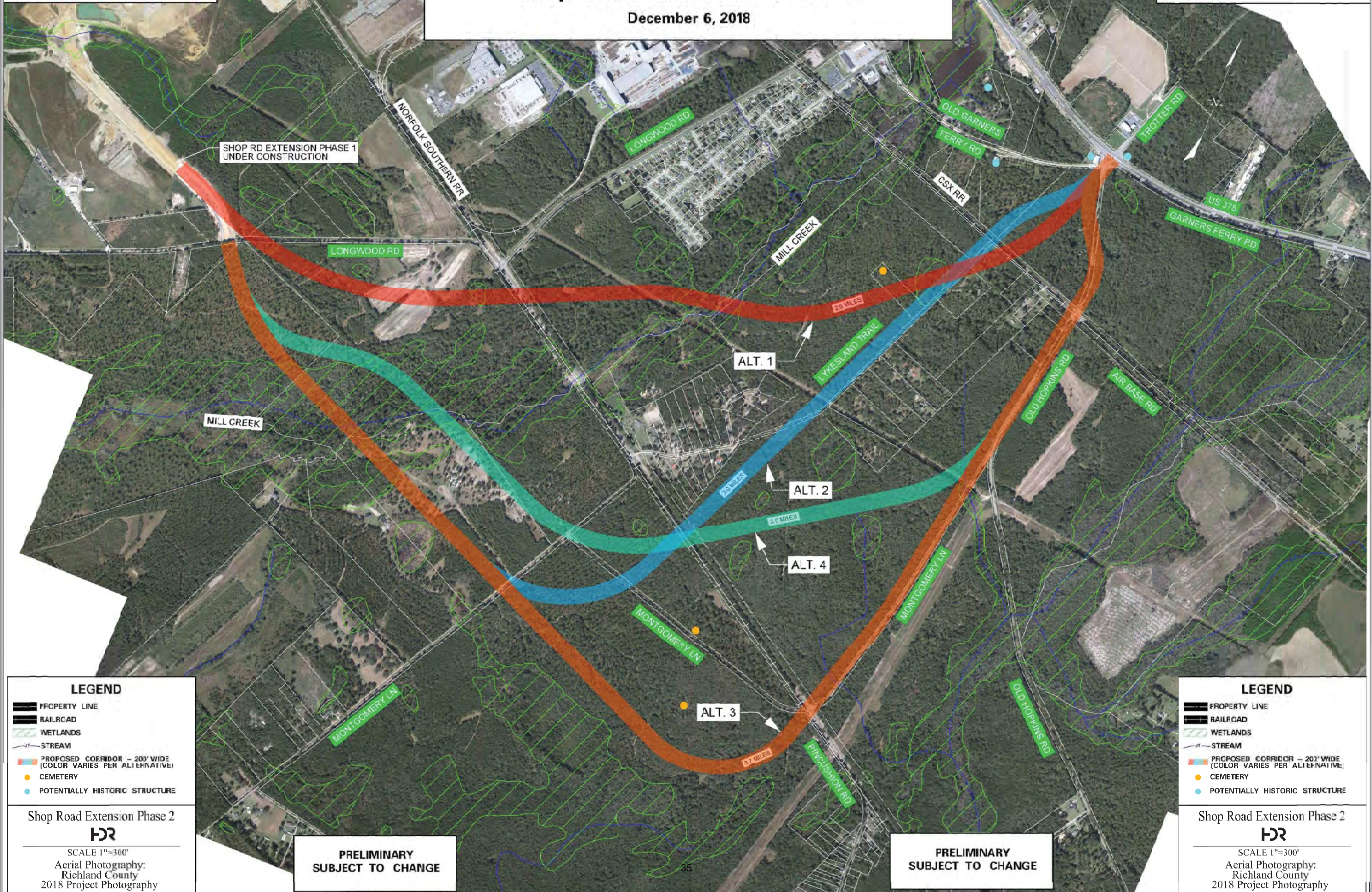
Exhibit B: December 6, 2018 Public Meeting Material

Exhibit C: Public Meeting Summary

PUBLIC INFORMATION MEETING

Shop Road Extension Phase 2

December 6, 2018



LEGEND

- PROPERTY LINE
- RAILROAD
- WETLANDS
- STREAM
- PROPOSED CORRIDOR - 200' WIDE (COLOR VARIES PER ALTERNATIVE)
- CEMETERY
- POTENTIALLY HISTORIC STRUCTURE

Shop Road Extension Phase 2

HR

SCALE 1"=300'

Aerial Photography:
Richland County
2018 Project Photography

**PRELIMINARY
SUBJECT TO CHANGE**

LEGEND

- PROPERTY LINE
- RAILROAD
- WETLANDS
- STREAM
- PROPOSED CORRIDOR - 200' WIDE (COLOR VARIES PER ALTERNATIVE)
- CEMETERY
- POTENTIALLY HISTORIC STRUCTURE

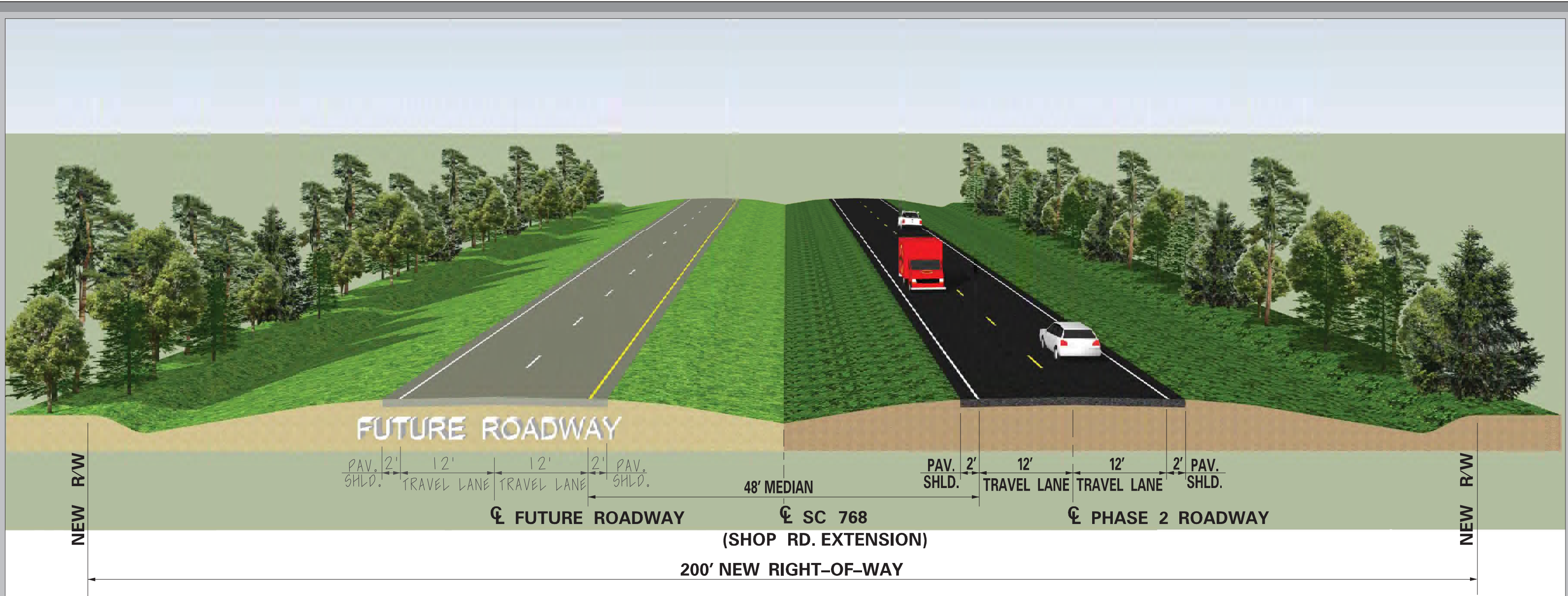
Shop Road Extension Phase 2

HR

SCALE 1"=300'

Aerial Photography:
Richland County
2018 Project Photography

**PRELIMINARY
SUBJECT TO CHANGE**



Shop Road Extension Phase 2 Typical Section

Public Information Meeting
December 6, 2018





TRANSPORTATION PROGRAM

EXECUTIVE SUMMARY

Date: 2/1/19

To: John Thompson, PE
Director of Transportation

From: David Beaty, PE
Program Manager

RE: McNulty Street Improvements - Public Meeting Summary and Recommendations

Introduction

The McNulty Street Improvements project is a one of the five (5) priority projects allocated for funding for the Town of Blythewood area in lieu of the Blythewood Road Widening project from Syrup Mill Road to Winnsboro Road (as approved by County Council in March 2015). The project proposes widening and improvements to McNulty Street from Blythewood Road to Main Street (US Route 21). The purpose of this document is to summarize the public input to date and provide recommendations to advance the project.

January 24th, 2019 Public Meeting

The Richland County Transportation Program held a Public Meeting for the McNulty Street Improvements project on Thursday, January 24th, 2019 from 5:00 to 7:00 p.m. at Doko Manor, 100 Alvina Hagood Circle in the Town of Blythewood, SC. The meeting was conducted with an informal, open house format where individuals were able to review project displays of the proposed alternatives typical sections and discuss questions with Richland County Transportation Program representatives. As people entered the meeting, staff provided a project handout and comment card and encouraged the public to provide comments on the proposed improvements and typical section. Refer to Exhibit A for the public meeting material, including proposed typical sections.

Forty-four (44) people attended the public meeting and a total of 18 written comments were received from the meeting and within the following two week comment period. The comments received were all in favor of Alternative B (no comments were received in favor of Alternative A). Other notable comments included the high regard for bike and pedestrian accommodations in the corridor, concerns with rights of way impacts and impacts to Town of Blythewood Historic Properties, safety issues at the intersection of McNulty Street and Blythewood Road, drainage issues along McNulty Street and the

addition of sidewalks along Boney Rd and the un-named street between McNulty and Boney Rd to provide pedestrian connectivity between McNulty Street and Blythewood Road. The public meeting summary is attached as Exhibit B and has further details on the input collected from the residents.

The project will again be presented to the public prior to rights-of-way acquisitions. This will allow the residents to view the selected alternate and discuss specific concerns with the Richland County and Richland Penny staff.

Recommendations

Based on the comments and input received at the public meeting as well as consideration of safety, project impacts, program intent and costs, the Alternate B typical section is recommended for the McNulty Street Improvements project. The typical section will include on-street bicycle lanes with curb and gutter and sidewalk on each side of the roadway. Refer to Exhibit A for a depiction of the recommended typical section. The roadway improvements will be designed to limit and reduce the amount and degree of impacts to commercial development, businesses, residences and historic properties and cognizant of existing utilities, within the corridor.

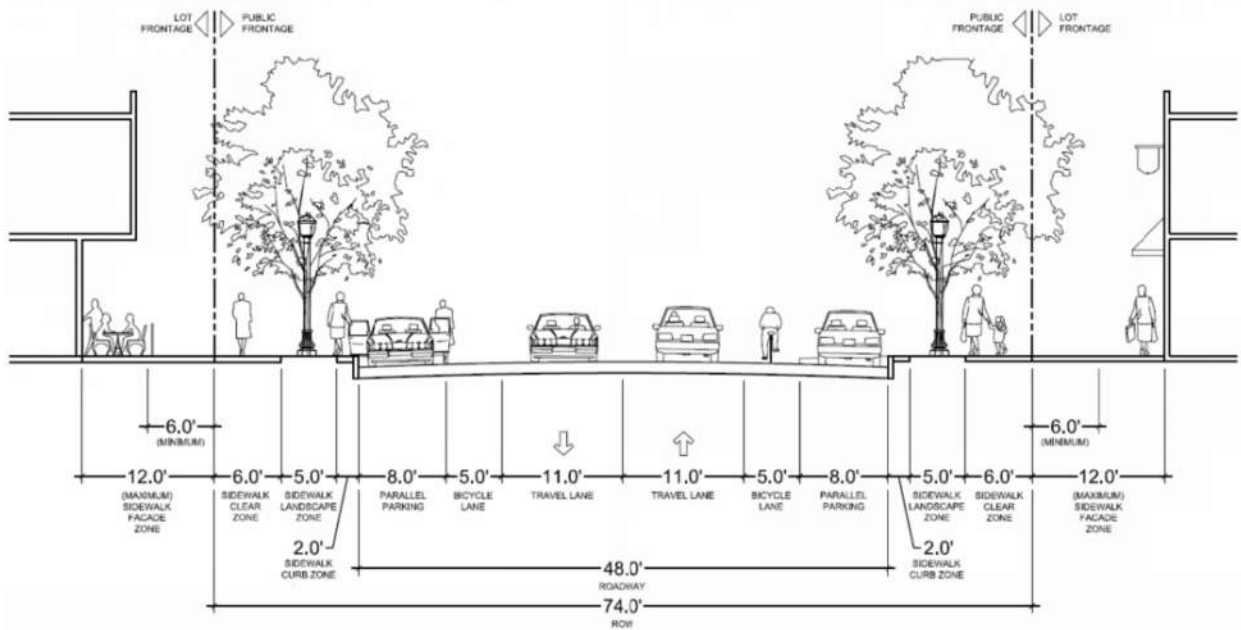
See below for a summary of the proposed recommendations for advancing this project.

Typical Section	Alternative B – (refer to Exhibit A for depiction)
Proposed Termini	McNulty Street- from Blythewood Road to Main Street (US 21).
Additional Improvements	Evaluate safety concerns / issues at the intersection of McNulty Street and Blythewood Road. Continue evaluation of roundabout design / feasibility and impacts at the intersection of McNulty Street and Boney Road Implement construction of pedestrian improvements along the “Unnamed Street” and Boney Road to provide pedestrian connectivity within the project area.

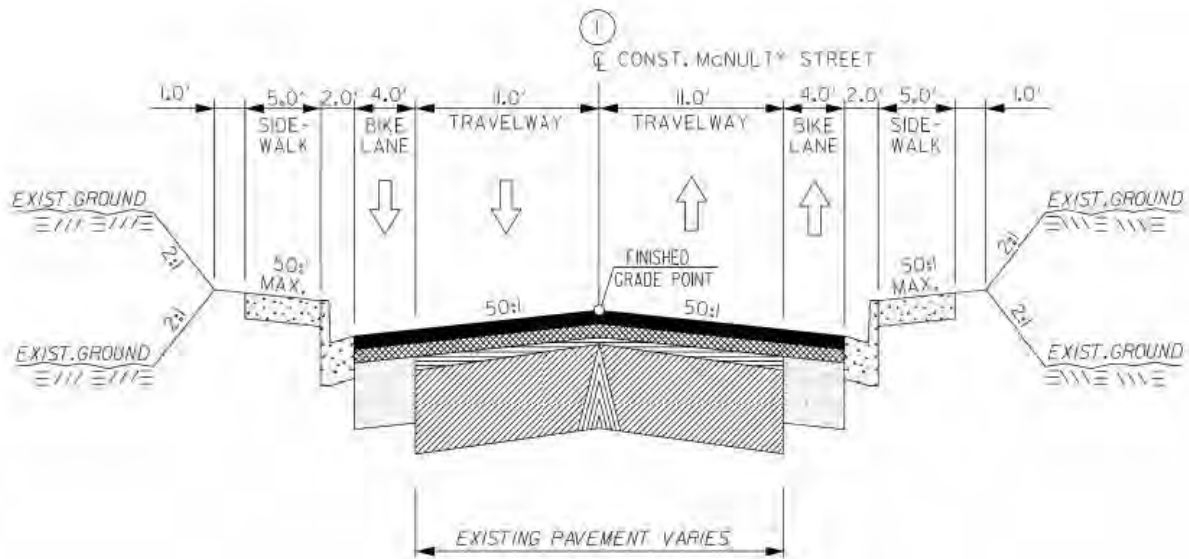
Exhibit A: January 24th, 2019 Public Meeting Material
Exhibit B: Public Meeting Summary



McNulty Street Improvements Project Limits (as presented at Public Meeting on January 24, 2019)



Alternate A Typical Section



Alternate B Typical Section

McNulty Street Improvements Typical Sections – Alternates A & B (as presented at Public Meeting on January 24, 2019)



McNulty Street Improvements Public Meeting Summary January 24th, 2019

Richland County (in coordination with the Richland County Penny Sales Tax Program Development Team) held the first public meeting for the McNulty Street Improvements project on Thursday, January 24th, 2019 from 5:00-7:00 p.m. at Doko Manor, 100 Alvina Hagood Circle in Blythewood, SC.

The meeting was advertised through road signs, public notice letters and flyers as well as media alerts. Road signs were placed at the intersections of Blythewood Rd and McNulty Street, Boney Rd and McNulty Street and Main Street and McNulty Street. Media alerts were distributed by County staff via the Richland County Facebook page.

The meeting was held in an open-house format. Residents were greeted at the venue entrance, checked in at a sign-in table, provided a handout and comment card and directed to the sets of project display boards, which were manned by program team members and project design consultants. Residents received handouts with project details and a comment card to provide feedback. Residents were able to review the conceptual alternatives and ask questions of the project design team members at the meeting. No formal presentation or address was made to the public. Comment card boxes were available and attendees were encouraged to provide their comments by the deadline of February 8, 2019. The number of comment cards and specific comments mentioned below are reflective of those comments received by the requested deadline of February 8, 2019.

Meeting Attendance (Signed in): 44

Comment Cards Left At Meeting: 16

Comment Cards Mailed: 0

Comments E-Mailed: 2

Total Comments and/or Comment Cards Received: 18



COMMENT SUMMARY

Residents and the general public provided feedback and questions specific to the McNulty Street Improvements project. Below is a general summary of the sentiments, concerns and specific ideas expressed by the public comments, including those received verbally by the project team. Copies of all comments received as of February 8, 2018 are attached to this document.

- **Typical Section**
 - Alternative A: No respondents provided a comment in favor of the Master Plan typical section (Alternative A) presented at the meeting.
 - Alternative B: All respondents who provided information relative to the typical section stated that Alternative B was preferred.
 - On-Street parking as presented in Alternative A was mentioned frequently as unnecessary for the corridor in terms of functionality, existing land-use and associated impacts.
 - High regard was given to sidewalks and pedestrian accommodations in the corridor. Bicycle accommodations were also stated as a positive measure. Some citizens suggested constructing sidewalks along Boney Rd and the unnamed street (from McNulty to Blythewood Rd) for connectivity to existing facilities.
- **R/W issues**
 - Many citizens / business owners / organization and / or agency representatives were concerned about the magnitude of rights of way impacts / loss of parking as reflected in the Alternative A plan view.
 - Many citizens were concerned about impacts to the Town of Blythewood historic properties (3) that exist along the corridor
- **Traffic**
 - Comments were received that school traffic along Boney Rd was problematic during drop-off / pick-up times.
 - Some residents provided negative response to the potential for a roundabout at the intersection of Boney Rd and McNulty Street. Other residents offered suggestion of 4-way stop control, leaving as-is, or a traffic signal. The roundabout provides improvements to the intersection operations and is recommended for further evaluation regarding cost and impacts.
 - Several comments were made relative to the dangers of the current intersection of McNulty and Blythewood Rd during peak traffic.
 - Several comments were made to attempt to utilize the existing unnamed street to serve some functionality in the proposed improvements.
- **General Comments**
 - Existing drainage issues along McNulty Street were mentioned.
 - Provisions for additional lighting and landscaping were mentioned.
 - Addressing directional signage issues for McDonald's, SubWay, Comfort Inn, etc with any changes to the McNulty/Blythewood Rd intersection
 - Impacts to the Town of Blythewood historic properties was highly discouraged.

Attachments: Public Meeting Sign-in Sheet & Public Comments (received at meeting and via email)

SERVICE ORDER MODIFICATION

Project No./Name: [178 Percival Rd Sidewalk](#)
Service Order No.: [No. 8](#)
Modification No.: [2](#)
Consultant: [Holt Consulting Company, LLC](#)
Modification Type: Contingency Authorization
 Other

RICHLAND COUNTY
 Department of Transportation
 P.O. Box 192
 2020 Hampton St.
 Columbia, S.C. 29201

DESCRIPTION:

To provide Subsurface Utility Engineering in order to assist with the design on the proposed 5'-0" wide concrete sidewalk along Percival Rd.

SERVICE ORDER BUDGET SUMMARY

	Amount
Consultant Compensation	\$113,446.00
Contingency	\$11,344.00
Total Service Order Budget	\$124,790.00

CONTINGENCY AUTHORIZATIONS

Contingency Budget	\$11,344.00
Authorized Contingency	\$88,558.00
Available Contingency	-\$77,214.00

MODIFICATION DETAILS

Mod. No.	Description	Contingency Authorization	Other
1	Nationwide Permit	\$11,310.00	
2	SUE Levels A, B and C	\$77,248.00	X
3			
4			
5			
6			
7			
8			
9			
10			

****Requested authorizations exceed Contingency Budget**** \$88,558.00



Accepted by: Richland PDT Project Manager (Signature)

12/19/2018

Date

Accepted by: Authorized Richland County Management (Signature)

Date

ATTACHMENT “A”
SCOPE OF SERVICES AND SCHEDULE
SC 12 (Percival Road) Sidewalk Project – SUE Services

Introduction

Holt Consulting, LLC (CONSULTANT) has been authorized by Richland County (**COUNTY**) to provide subsurface utility engineering services to assist with the plan development of pedestrian accommodations along (SC 12) Percival Road, from Forest Drive (SC 12) to Northshore Road (S-1583) in Richland County, South Carolina.

This scope of services and schedule is for the completion of SUE Level A, B, and C Services based on the Percival Road plans.

Project Locations - The project is in Richland County, including parts within the cities of Columbia and Forest Acres. The sidewalk will be constructed between Forest Dr. and Northshore Rd.

Existing Conditions – Percival Road within the project area is a majority two lane shoulder section and valley gutter roadway with widening at the intersection with Forest Drive and at Decker Blvd. to accommodate turning lanes. Minimal sections of curb and gutter and sidewalk exist at the project termini with Forest Drive and Decker Blvd.

Proposed Project Scope – Subsurface Utility Engineering will be performed in order to assist with design implementation of the project.

The proposed improvements consist of constructing a new sidewalk (5’-0”) along the western (southbound) side of Percival Road, from Forest Drive to Northshore Road, for approximately 1.40 miles.

Summary of Anticipated Services - An outline of the services anticipated for this project is shown below.

Task 8 – Subsurface Utility Engineering (SUE)

Task 8

SUBSURFACE UTILITIES ENGINEERING (SUE)

Within fifteen (15) days of Notice to Proceed for the contract, the **CONSULTANT** will provide the **COUNTY** with a recommendation as to the extent of SUE services to be provided. This should include as much information as can be assembled on utility type, approximate location, owner, material type, prior rights, and any preliminary assessment of impact with respect to the scope of the proposed project. This information will be used to specifically define the limits of the SUE work to be performed.

The **CONSULTANT** shall perform work in two phases. The first phase consists of designating services (Quality Level B and C). For the purpose of this agreement, “designate” shall be defined as indicating (by marking) the presence and approximate horizontal position of the subsurface utilities by the use of geophysical prospecting techniques. The second phase consists of test hole services (Quality Level A). For the purpose of this agreement, “locate” means to obtain the accurate horizontal and vertical position of the subsurface utilities by excavating a test hole. The **CONSULTANT** shall provide these services as an aide in the design of right-of-way and construction plans for the project.

Unless specifically stated otherwise, the **CONSULTANT** shall adhere to the ASCE Standard Guideline for the Collection and Depiction of Existing Subsurface Utility Data (CI/ASCE 38-02).

Designating shall be estimated on a cost per linear foot basis and shall include all labor, equipment, and materials necessary to provide complete SUE plans. Locating shall be estimated on a per each basis and shall include all labor, equipment, and materials necessary to provide complete SUE plans. Direct charges for mileage, meals, lodging, reproductions shall be shown separately. Traffic control shall be estimated on a per day basis and shown separately. No separate payment will be made for mobilization and should be included in the per linear foot or per each price for designating or locating. It is assumed that two (2) total mobilizations will be required by the **CONSULTANT**.

Designating –

A. In the performing of designating services under this agreement, the **CONSULTANT** shall,

1. Provide all equipment, personnel and supplies necessary for the completion of Quality Level B information for approximately 37,500 LF of underground utilities.
2. Provide all equipment, personnel and supplies necessary for the completion of Quality Level C information for approximately 22,000 LF of underground utilities.
3. Provide all equipment, personnel, and supplies necessary for the accurate recording of information for approximately 0 LF of aerial utilities. *The estimation of aerial utilities is measured from power pole to power pole and includes all utility lines attached to the poles.*

4. Conduct appropriate records and as-built plans research and investigate site conditions. Digital copies of records and as-built plans research to be provided to **COUNTY**.
5. Obtain all necessary permits from city, county, state or any other municipal jurisdictions to allow **CONSULTANT** personnel to work within the existing streets, roads and rights-of way.
6. Designate the approximate horizontal position of existing utilities by paint markings or pin flags in accordance with the APWA Uniform Color Code scheme along the utility and at all bends in the line in order to establish the trend of the line. All utilities shall be designated as well as their corresponding lateral lines up to the point of distribution, existing right-of-way limits, or whichever is specifically requested and scoped for each individual project.
7. Survey designating marks, which shall be referenced to project control provided by the surveyor of record.
8. Draft survey information using DEPARTMENT CADD guidelines for Subsurface Utility Engineering consultants (latest version).
9. Final review and seal of all appropriate work by a professional engineer and/or land surveyor licensed in South Carolina in responsible charge of the project.

B. In the performing of designating services under this agreement, the **COUNTY** shall,

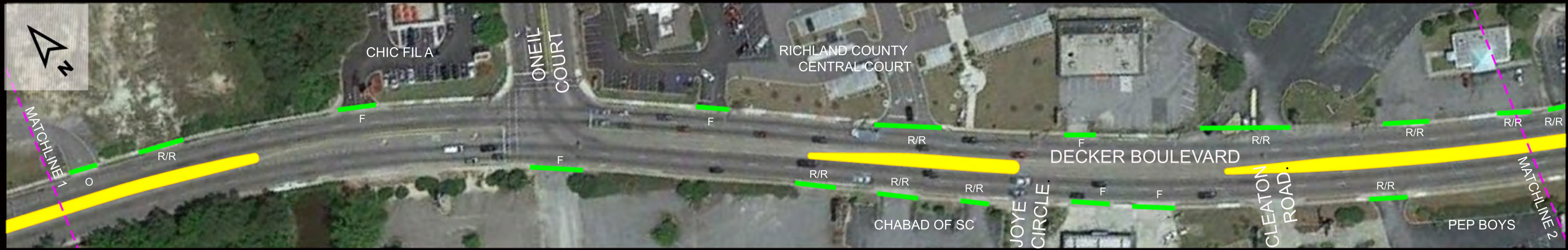
1. When requested, provide reasonable assistance to the **CONSULTANT** in obtaining plans showing the project limits, alignment, centerline, rights-of-way limits (existing and proposed), project controls and other data for selected projects.
2. Provide notification to key **DEPARTMENT** District personnel concerning the upcoming SUE services to be provided by the **CONSULTANT**.

The above quantities are based on the **CONSULTANT** performing Level B and C SUE services within the following area: SC 12 (Percival Road) from STA 12+00 to STA 86+50 on project plans provided (roughly from north of Forest Drive to the north side of the Northshore Drive intersection). SUE services will be performed only between the center of Percival Road and the northwestern right-of-way line along this corridor. Level B SUE will be attempted on water, gas, electric, sanitary sewer force main, and utilities found to be in conduit. Level C SUE will be performed for direct-buried communication and any utilities which the **CONSULTANT** attempts to perform Level B SUE on but is unsuccessful. The **CONSULTANT** will not mark or map private service lines for power, telephone, or television. The **CONSULTANT** will map water and gas service lines to the right-of-way or meter, whichever is closest to the main. Expected quantities of each utility are: Level B: 13,000 LF Water, 12,000 LF Gas, 2,000 LF Electric, 500 LF Force Main, 10,000 LF Communications Conduit; Level C: 20,000 LF Telephone, 2,000 Television. The **CONSULTANT** will notify the **COUNTY** immediately should additional SUE be recommended. The **CONSULTANT** will notify the **COUNTY**'s designated Project Manager prior to performing any work on site.

Locating–

A. In the performance of locating services under this agreement, the **CONSULTANT** shall,

1. Provide all equipment, personnel and supplies necessary for the completion of Quality Level A information for an estimated 10 test holes.
 2. Conduct appropriate records and as-built research and investigate site conditions.
 3. Obtain all necessary permits from city, county, state or any other municipal jurisdictions to allow **CONSULTANT** personnel to work within the existing streets, roads and rights-of-way.
 4. Perform electronic or ground penetrating radar sweep of the proposed conflict and other procedures necessary to adequately “set-up” the test hole.
 5. Excavate test holes to expose the utility to be measured in such a manner that insures the safety of excavation and the integrity of the utility to be measured. In performing such excavations, the **CONSULTANT** shall comply with all applicable utility damage prevention laws. The **CONSULTANT** shall schedule and coordinate with the utility companies and their inspectors, as required, and shall be responsible for any damage to the utility during excavation.
 6. Provide notification to the **COUNTY** concerning 1) the horizontal and vertical location of the top and/or bottom of the utility referenced to the project survey datum; 2) the elevation of the existing grade over the utility at a test hole referenced to the project survey datum; 3) the estimated outside diameter of the utility and configuration of non-encased, multi-conduit systems; 4) the utility structure material composition, when reasonably ascertainable; 5) the benchmarks and/or project survey data used to determine elevations; 6) the paving thickness and type, where applicable; 7) the general soil type and site conditions; and 8) such other pertinent information as is reasonable ascertainable from each test hole site.
 7. When an attempt to test hole a utility line does not provide valid horizontal and vertical data, the test hole shall not be reimbursable by the **COUNTY**.
 8. Provide permanent restoration of pavement within the limits of the original cut. When test holes are excavated in areas other than roadway pavement, these disturbed areas shall be restored as nearly as possible to the condition that existed prior to the excavation.
 9. Draft horizontal location and, if applicable, profile view of the utility on the project plans using CADD standards as outlined above. A station and offset distance and/or northing and easting coordinates (State Plane) with elevations shall be provided with each test hole.
 10. Test hole information shall be formatted and presented on **CONSULTANT**'s certification form and listed in a test hole data summary sheet.
 11. Certification form shall be reviewed and sealed by a professional engineer and/or land surveyor licensed in South Carolina and in responsible charge of the project.
- B. In the performance of locating services under this agreement, the **COUNTY** shall,
1. When requested, provide reasonable assistance to the **CONSULTANT** in obtaining plans showing the project limits, alignment, centerline, rights-of-way limits (existing and proposed), project controls and other data for selected projects.
 2. Provide notification to key **DEPARTMENT** District personnel concerning the upcoming SUE services to be provided by the **CONSULTANT**.



LANDSCAPED MEDIAN LOCATION 

DRIVEWAY (OPEN) 

F (FULL ACCESS)

R/R (RIGHT IN/RIGHT OUT)

O (ONE WAY)

NOTE: NO DRIVEWAY CLOSURES ARE PROPOSED.

**DECKER BOULEVARD
PROPOSED LANDSCAPED MEDIAN LOCATIONS
TRENHOLM ROAD EXT. TO BROOKFIELD ROAD
COLUMBIA, SC**



TRANSPORTATION PROGRAM





DRIVEWAY MODIFICATION █

DRIVEWAY (OPEN) █

F (FULL ACCESS)

R/R (RIGHT IN/RIGHT OUT)

O (ONE WAY)

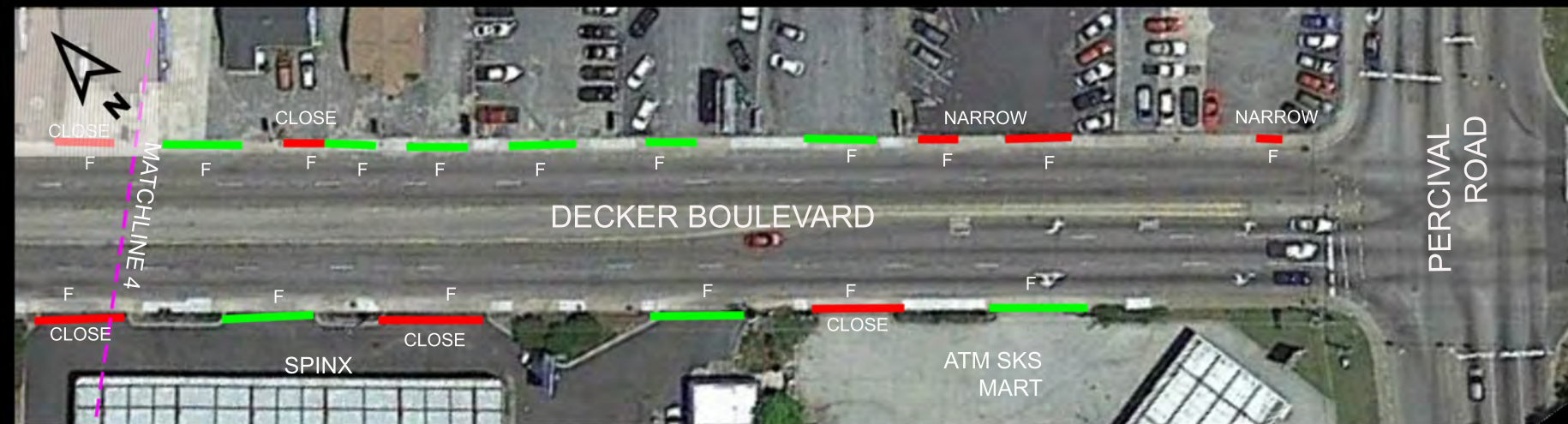
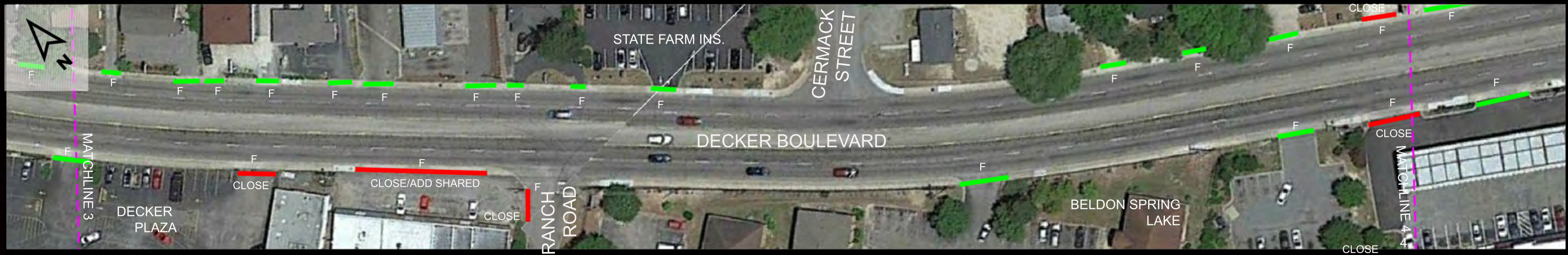
NOTE: NO LANDSCAPED MEDIANS ARE PROPOSED.

**DECKER BOULEVARD
PROPOSED DRIVEWAY MODIFICATIONS
BROOKFIELD ROAD TO PERCIVAL ROAD 1
COLUMBIA, SC**





TRANSPORTATION PROGRAM





NOTE: NO LANDSCAPED MEDIANS ARE PROPOSED.

DRIVEWAY MODIFICATION	
DRIVEWAY (OPEN)	
F	(FULL ACCESS)
R/R	(RIGHT IN/RIGHT OUT)
O	(ONE WAY)

DECKER BOULEVARD
 PROPOSED DRIVEWAY MODIFICATIONS
 BROOKFIELD ROAD TO PERCIVAL ROAD 2
 COLUMBIA, SC



TRANSPORTATION PROGRAM



APPENDIX 1 – MAINTENANCE PARTNERSHIP AGREEMENT

2/17/00

THIS AGREEMENT is entered this _____ day of _____, 20__, by and between Richland County, hereinafter referred to as County, and the South Carolina Department of Transportation, hereinafter referred to as SCDOT.

WHEREAS, in accordance with Sections 57-3-110 (1) and (10), 57-3-650, 57-23-10, 57-23-800(E), 57-25-140, and the SCDOT's Policy of Vegetation Preservation on SC Highways, SCDOT is authorized to allow landscaping and beautification efforts on SCDOT right of ways;

WHEREAS, the County has previously obtained a Cooperative Intergovernmental Agreement (IGA) for the coordination of the Richland County Sales Tax Transportation Program to access SCDOT's right of way for construction and/or improvement of transportation facilities. Said IGA is described as follows:

IGA Number: 25-14 Date Issued: February 7, 2014

Location: Blythewood Road from I-77 to Syrup Mill Road;

WHEREAS, SCDOT and the County are desirous of entering into this Agreement to grant a continuous license to the County to enter the SCDOT's right of way to conduct routine maintenance of landscaping, beautification and/or enhancements permitted by the aforesaid IGA;

NOW THEREFORE, in consideration of mutual promises, SCDOT and the County agree to the following:

1) SCDOT grants the County a license to enter onto the SCDOT right of way at the area defined by the intergovernmental agreement. The purpose of the license to enter is limited to routine maintenance of the intergovernmental agreement area. Such entry will be limited to the scope of the work identified in the intergovernmental agreement. No additional encroachment beyond that contemplated by the original intergovernmental agreement is allowed. If additional maintenance, enhancement and/or beautification efforts, different from the original scope of work identified in the intergovernmental agreement, is requested, the County will be required to submit a new intergovernmental agreement identifying the new scope of work. Entry onto SCDOT right of way pursuant to this agreement may be without notice to the SCDOT.

2) The County agrees to post all necessary traffic control devices and take all necessary precautions in conformance with SCDOT traffic control standards and as required by the SCDOT, along the SCDOT right of way prior to and during the performance of any routine maintenance, enhancement and/or beautification efforts.

3) SCDOT agrees to accept maintenance responsibilities for the shared use path concrete structure not to include cleaning or hazardous weather maintenance of the surface.

4) The County agrees to accept maintenance responsibilities for maintenance of the shared use path's surface to include cleaning and hazardous weather maintenance of the surface.

5) The County agrees to maintain the vegetation zone located between the edge of roadway and the shared use path as well as the vegetation zone on the outside shoulder of the path. This maintenance includes, but is not limited to, mowing and clearing/limbing vegetation management.

6) The County agrees to accept maintenance responsibilities for all handrails constructed as part of the project.

7) The County agrees to be responsible for all claims or damages arising from the work performed by the County, its employees or agents, but only within the limits of the SC Tort Claims Act. In

addition, should the County use a contractor for performance of the work, the County shall insert a hold harmless and indemnification clause in its contract with all contractors and subcontractors which requires the contractor and subcontractor to indemnify and hold harmless the County and the State of South Carolina, specifically the SCDOT, from any liability, claims or damages which may arise from the performance of the work on SCDOT right of way. Nothing in this Agreement shall be construed to expand County liability for its actions in SCDOT's right of way beyond the limits of the S. C. Tort Claims Act. Further, the County agrees that they are subject to S. C. Code Section 57-5-140, which provides that SCDOT shall not be liable for damages to property or injuries to persons, as otherwise provided for in the Torts Claims Act, as a consequence of the negligence by a municipality in performing such work within the State highway right of way.

8) This Agreement shall not be modified, amended or altered except upon written consent of the parties. Neither party shall assign, sublet, or transfer its interest in this Agreement without the written consent of the other.

9) This Agreement may be terminated upon thirty days' written notice to the other party; however, in cases where the County is not performing in accordance with this Agreement, SCDOT shall give written notice to the County of the failure in performance and, if the County does not correct or cure the performance within three days of receipt of the notice, SCDOT shall have the option to terminate this license immediately, and shall, thereafter, give written notice of such termination to the County.

IN WITNESS HEREOF, the above parties have hereunto set their hands and seals.

SOUTH CAROLINA DEPARTMENT OF
TRANSPORTATION

RICHLAND COUNTY

By: _____
Its: _____
Recommended by: _____

By: _____
Its: _____



TRANSPORTATION PROGRAM

Date: 2/13/19

To: Dr. John M. Thompson, Ph.D., MBA, CPM
Director of Transportation

From: David Beaty, PE
Program Manager

RE: Blythewood Area Improvements – Recommended Approval of Town of Blythewood Proclamation of Priority Listing of Projects.

Recommendation: It is recommended to approve the proclamation as provided by the Town of Blythewood, dated January 28th, 2019, providing a definitive priority listing of the projects to be funded within the Blythewood area.

Background

The 2012 Richland County Penny Sales Tax Referendum included (2) widening's within the Blythewood area (both were widening's along Blythewood Rd)

1. Syrup Mill to I-77
2. Winnsboro Rd to Syrup Mill.

The project list included in the referendum reflected a statement at the bottom of the page that reads, "*Town of Blythewood to provide input on its projects*". – See Exhibit A

Therefore, in 2015, prior to any project development on any of the projects in Blythewood, the Town developed a resolution requesting to move forward with the 1st Blythewood Rd Widening (Syrup Mill to I-77), but to remove the 2nd widening and instead fund other projects within the Town District. This resolution was passed by Blythewood Town Council in May 2014 and then approved by County Council in March 2015. The amended projects (to take the place of the 2nd widening) were in no particular priority order at the time and in no way defined by any order in the original resolution. The projects included in this resolution were deemed as more of a need for the Town of Blythewood than the widening of Blythewood Rd (from Winnsboro Rd to Syrup Mill). – See Exhibit B.

Blythewood Town Council approved the listing of the projects included in the resolution (as previously approved by County Council in March 2015) in March 2018 at their monthly Town Council meeting. The resolution stated that McNulty Street Improvements was their

first priority. At the time of this statement, the remaining projects did not receive a specific priority ranking. – See Exhibit C

Blythewood Town Council passed a resolution in January 2019 to formally prioritize those projects as approved by County Council in 2015 (this is the document that was included in the Council package at their meeting on Feb 5th). The priority list actually removes one project from the listing that was approved in 2015; it has been determined that this project would be costly and very impactful and therefore, the Town decided it was not necessary. The priority list provided to Council would then finalize the resolution as approved by Council in 2015 by definitively stating a priority for these projects. – See Exhibit D

Therefore, with this information, this latest resolution does not change anything that has been previously approved by County Council; it only amends the resolution to provide a definitive path forward for those previously approved projects in terms of priority for project development.

Exhibit A: 2012 Sales Tax Referendum Project List

Exhibit B: March 17, 2015 County Council Minutes approval of Town Resolution

Exhibit C: March 26, 2018 Town Council Meeting Minutes Prioritizing McNulty Street Improvements

Exhibit D: January 28, 2019 Town Council Resolution prioritizing the approved Project List

2012 Roadway Projects				
Type	Project Name	Begin Location	End Location	Total
Widening	Pineview Rd	Bluff Rd	Garners Ferry Rd	\$18,200,000
Widening	Atlas Rd	Bluff Rd	Garners Ferry Rd	\$17,600,000
Widening	Clemson Rd	Old Clemson Rd	Sparkleberry Crossing Rd	\$23,400,000
Widening	Hardscrabble Rd	Farrow Road	Lake Carolina Blvd	\$29,860,800
Widening	Blythewood Rd	Syrup Mill Rd	I-77	\$8,000,000
Widening	Lower Richland Blvd	Rabbit Run Rd	Garners Ferry Rd	\$6,100,000
Widening	Broad River Rd	Royal Tower Rd	I-26 (Exit 97)	\$29,000,000
Widening	Shop Rd	I-77	George Rogers Blvd	\$33,100,000
Widening	Polo Rd	Mallet Hill Rd	Two Notch Rd	\$12,800,000
Widening	Bluff Rd	I-77	Rosewood Dr	\$16,700,000
Widening	Blythewood Rd	Winnsboro Rd	Syrup Mill Rd	\$21,000,000
Widening	Spears Creek Church Rd	Two Notch Rd	Percival Rd	\$26,600,000
Widening	North Main Street (Phases IA2 & III; II & IV)	Anthony Avenue	Fuller Avenue	\$30,000,000
Widening	Leesburg Road	Fairmont Rd	Lower Richland Blvd	\$4,000,000
Special	Shop Road Extension*	na	na	\$71,800,000
Special	Kelly Mill Rd.**	na	na	\$4,500,000
Special	Innovista Transportation-Related Projects ***	na	na	\$50,000,000
Special	Riverbanks Zoo Transportation-Related Projects ****	na	na	\$4,000,000
Special	Neighborhood Improvement Transportation Projects	County wide	County wide	\$63,000,000
Special	Commerce Drive Improvements	Royster Street	Jim Hamilton Boulevard	\$5,000,000
Special	Assembly Street RR Grade Separation	na	na	\$0
Intersection	Summit Pkwy and Summit Ridge Rd.	Summit Pkwy	Summit Ridge Rd.	\$500,000
Intersection	Clemson Rd. and Rhame Rd./North Springs Rd.	Clemson Rd.	Rhame Rd./North Springs Rd.	\$3,500,000
Intersection	Farrow Rd. and Pisgah Church Rd.	Farrow Rd.	Pisgah Church Rd.	\$3,600,000
Intersection	Wilson Blvd. and Pisgah Church Rd.	Wilson Blvd.	Pisgah Church Rd.	\$3,600,000
Intersection	North Main St. and Monticello Rd.	North Main St.	Monticello Rd.	\$5,400,000
Intersection	Broad River Rd. and Rushmore Rd.	Broad River Rd.	Rushmore Rd.	\$3,700,000
Intersection	Wilson Blvd. and Killian Rd.	Wilson Blvd.	Killian Rd.	\$2,600,000
Intersection	Garners Ferry Rd. and Harmon Rd.	Garners Ferry Rd.	Harmon Rd.	\$2,600,000
Intersection	Clemson Rd. and Sparkleberry Ln. (to Mallet Hill Rd.)	Clemson Rd.	Sparkleberry Ln. (to Mallet Hill Rd.)	\$5,100,000
Intersection	North Springs Rd. and Risdon Way	North Springs Rd.	Risdon Way	\$1,800,000
Intersection	Hardscrabble Rd. and Kelly Mill Rd./Rimer Pond Rd.	Hardscrabble Rd.	Kelly Mill Rd./Rimer Pond Rd.	\$3,000,000
Intersection	Bull St. and Elmwood Ave.	Bull St.	Elmwood Ave.	\$2,000,000
Intersection	Screaming Eagle Rd. and Percival Rd.	Screaming Eagle Rd.	Percival Rd.	\$1,000,000
Intersection	Kennerly Rd. and Coogler Rd./Steeple Ridge Rd.	Kennerly Rd.	Coogler Rd./Steeple Ridge Rd.	\$1,900,000
Intersection	North Springs Rd. and Harrington Rd.	North Springs Rd.	Harrington Rd.	\$2,000,000
Interchange	I-20 / Broad River Rd.*****	I-20 / Broad River	I-20 / Broad River	\$52,500,000
Program	Local Road Resurfacing Program	County wide	County wide	\$40,000,000
Program	Dirt Road Paving Program	County wide	County wide	\$45,000,000
Program	Access Management & Complete Streets Initiatives	County wide	County wide	\$94,536
Program	County-Wide Corridor Improvement Plan	County wide	County wide	\$189,072
Program	County-Wide Thoroughfare Plan	County wide	County wide	\$189,072
Program	County-Wide HOV Lane Study	County wide	County wide	\$141,804
Program	Intelligent Transportation System	County wide	County wide	\$945,360
Included in Projects List: No Costs Associated (Some may not involve costs, while others may be included in Admin Costs)				
Special	Study of Outer Beltway	na	na	
Program	Preservation of Existing Right-of-Way	na	na	
Program	Extension of Existing Roads	na	na	
Program	Reservation of Road Connections	na	na	
Program	Transfer of Development Rights	na	na	
Program	Capital Improvements Plan	na	na	
Program	Traffic Mitigation Plans	na	na	
Program	Demand Management	na	na	
Program	Establish the Position of Director of Transportation	na	na	
Program	Update the County Zoning Ordinance	na	na	
Program	Encourage Transit Oriented Development	na	na	
Program	Encourage Traditional Neighborhood Development	na	na	
Total Roadway Projects				\$656,020,644

Notes:

*Shop Road Extension: Any funds budgeted but not expended for the Shop Road Extension project shall be used for local road resurfacing projects and / or local dirt road paving projects.

**This special project is from the intersection of Hardscrabble Road and Kelly Mill Road to the Lake Carolina Elementary School along Kelly Mill Road. The beginning would be near Hardscrabble Road and Kelly Mill intersection and end past the entrance to the Lake Carolina Elementary School.

***Innovista Transportation-Related Projects: The top two transportation-related priorities associated with Innovista are Greene Street from Assembly west to the to-be-constructed Williams Street Extension (aka Congaree River Parkway). [Further description of projects below.]

(1) Greene Street will consist of road improvements running west from Assembly to the railroad cut (1,600 linear feet); then the to-be-constructed Greene Street Bridge over the railroad cut; then from the Greene Street Bridge to Huger Street (900 linear feet); and then Greene Street from Huger Street to the to-be-constructed Williams Street Extension (300 linear feet). Also included in this project will be pedestrian sidewalks and bike lanes the length of Greene Street, significant improvements to the intersection of Greene Street and Lincoln Street which, among other matters, will improve the traffic flows in and around the Colonial Center; and a pedestrian promenade to be located to the west of the Greene Street Bridge to Huger Street and from Huger Street to the to-be-constructed Williams Street Extension.

(2) Williams Street Extension / Congaree River Parkway will consist of a new roadway from Blossom Street north to Gervais Street consisting of 2,650 linear feet as well as completing a section of Senate Street from the new roadway to the west. This project will also entail the relocation of power lines and gas lines.

****Riverbanks Zoo Transportation-Related Projects: Improvements would address Interstate 126 at Greystone Boulevard.

*****Any savings from Broad River Road / I-20 Interchange project will be applied to the Broad River Road Corridor improvements.

Other: Widening of Ridgewood / North Main Extension (Columbia portion) from Dixie Avenue to North Main Street was removed from the projects list, as no funding is required since this project will be funded by the City. Intersection of Lake Murray Boulevard and Kinley Road was removed, as improvements have been completed. **Town of Blythewood to provide input on its projects.** Emphasis to be placed on local / small / minority firms. A process is to be developed to ensure participation by these firms. A partnership with DOT is recommended. The type and level of partnership is TBD. An in-house Transportation Director was approved. The recommendation to procure outside Program / Project Management firm(s) was approved. An oversight / accountability / "watchdog" committee was approved. Membership / duties of this Committee TBD.

RICHLAND COUNTY COUNCIL

SOUTH CAROLINA

Richland County Council
Regular Session
Tuesday, March 17, 2015
Page Ten



- d. **Town of Blythewood Project Revision Request** – Mr. Livingston stated the committee recommended approval of the removing the Blythewood Widening Project and replacing it with the projects presented by the Town of Blythewood included in the Council agenda.

Mr. Malinowski asked if the resolution supporting the substitution of projects was approved unanimously and if citizen's input was allowed on this matter.

Mr. Perry will bring the answers back to Mr. Malinowski.

The vote in favor was unanimous.

Ms. Dickerson moved, seconded by Mr. Manning, to reconsider this item. The motion failed.

- e. **TPAC Mission** – Mr. Livingston stated the committee recommended sending the TPAC members the mission statement with a survey asking if they intend to continue serving under the current mission statement. The vote in favor was unanimous.

REPORT OF THE SEWER AD HOC COMMITTEE

- a. **Future Direction of Utilities**

1. **Richland County should explore the option of having a private company promote water service to a portion of Richland County whereby Richland County will benefit financially [RUSH and MALINOWSKI]** – Mr. Washington stated the committee recommended to direct staff to hire a consultant to update the water and sewer master plans. The vote in favor was unanimous.

- b. **Lower Richland Sewer** – Mr. Washington stated the committee recommended to direct staff to move forward with the citizen survey and to identify funding in order to waive all tap and connections fees prior to construction.

Mr. Malinowski requested a copy of the FOIA request, as well as, when they were received; if the FOIA requests were not responded to, why not; and how long the tap fees will be waived for the citizens.

Mr. Hammett stated funds have been identified to waive tap fees and connect up to 224 households. If the number exceeds that there are potential options to use additional CDBG funds and/or additional loans funds.

STATE OF SOUTH CAROLINA)
TOWN OF BLYTHEWOOD)

RESOLUTION NO. 2014.005

A RESOLUTION TO RECOMMEND AN ALTERNATIVE ROAD DEVELOPMENT PLAN TO RICHLAND COUNTY AS PART OF THE TRANSPORTATION PENNY PROGRAM

Whereas, Richland County TPAC has prepared a project list for road improvement projects which includes the widening of Blythewood Road from Winnsboro Road to Syrup Mill Road; and,

Whereas, Blythewood Town Council has simultaneously prepared a Town Master Plan to include the development of the Town Center District of Blythewood; and,


Whereas, the DOT traffic census figures 2006 to 2012 for the Blythewood area indicate no significant change in average annual daily traffic volumes in the past five years; and,

WHEREAS, it appears necessary and desirable to declare Town Council's preferences for the road improvement program.

NOW THEREFORE, BE IT RESOLVED by Blythewood Town Council, in council duly assembled this 21 day of May, 2014, as follows:

Blythewood Town Council approves the road development plan to widen Blythewood Road from Fulmer Road to Syrup Mill (and not Winnsboro Rd to Syrup Mill), for an estimated cost of \$10.7m and the resulting saving of \$10.3m be applied to road improvements for McNulty St, the proposed Creech Connector, I-77 to Main and traffic circles at Blythewood Rd/Creech Rd and Blythewood Rd/Cobblestones. The remaining Blythewood projects on Richland County's list for the Transportation Penny are unchanged.

ATTEST:



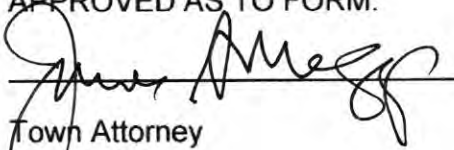
Mayor

5/29/2014



Town Administrator

APPROVED AS TO FORM:

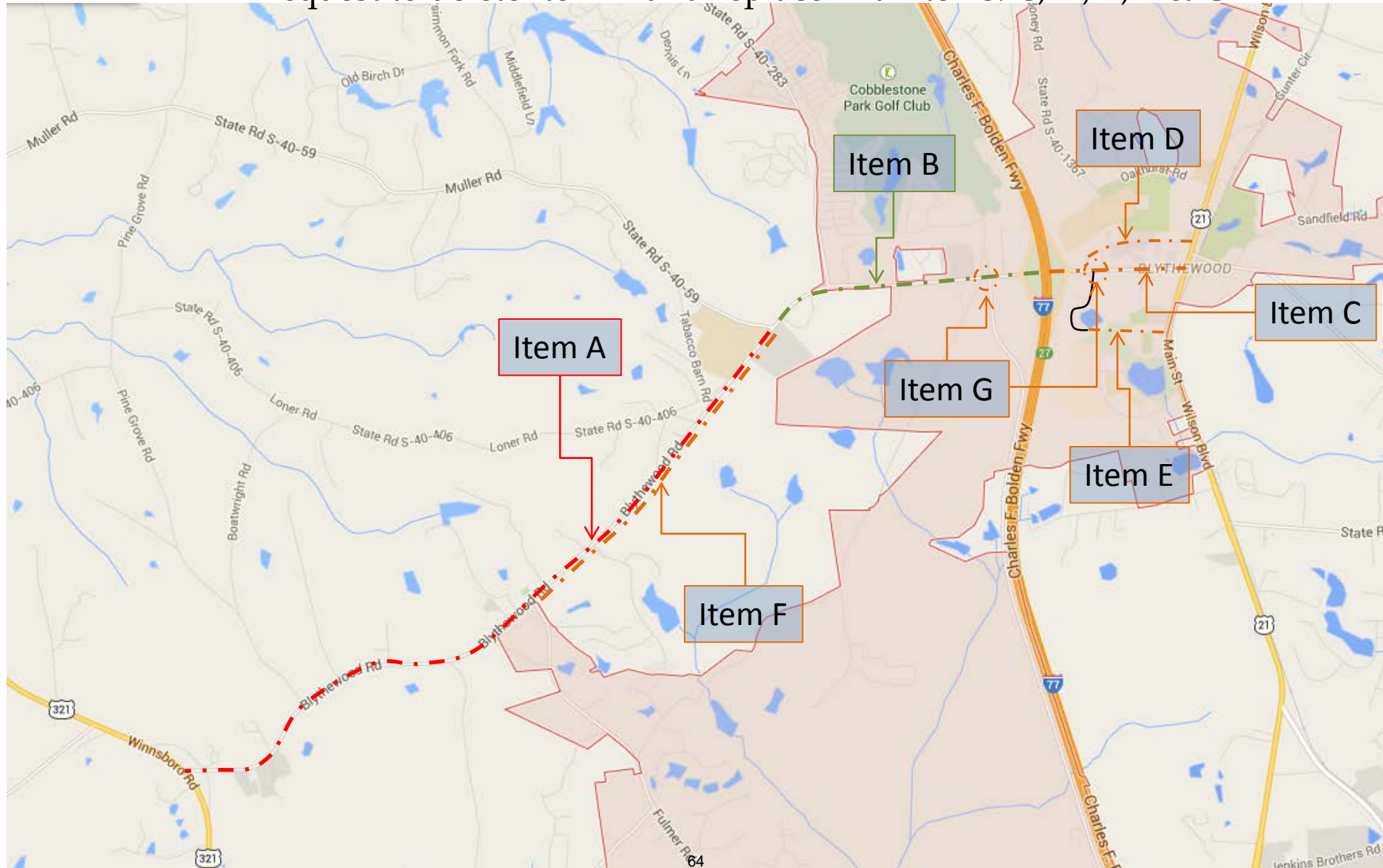

Town Attorney

BLYTHEWOOD HIGHWAYS Town Plan compared to Richland County TPAC - March, 2014

Ref	Segment	Distance - m	Cost/m	Total \$M	Comment	Nett \$M
A	B'wood Rd from Winnsboro Rd to Syrup Mill	3.38	6.21	21.0	3-lane: on list, not required	-21
B	Blythewood Rd from Muller Rd to I-77	0.88	9.09	8.0	5-lane: on list, required	0
C	Blythewood Rd from I-77 to Main	0.29	10.4	3.0	5-lane: not on list, required	3
D	McNulty from Main to Blythewood Rd	0.35	6.21	2.2	3-lane: not on list, required	2.2
E	Creech Rd extension to Main	0.29	6.21	1.8	3-lane: not on list, required	1.8
F	Blythewood Rd from Fulmer to Syrup Mill	1.72	6.21	10.7	3-lane: not on list, required	10.7
G	Traffic Circles at B'wood Road and Creech, and B'wood Rd and Cobblestone	n/a	n/a	3	Not on list, required Difference	3
	-0.3					

Town of Blythewood

Request to delete Item A and replace with items: C, D, E, F & G



BLYTHEWOOD TOWN COUNCIL MEETING

**THE MANOR
100 ALVINA HAGOOD CIRCLE
BLYTHEWOOD, SC 29016
MONDAY, MARCH 26 2018
7:00PM**

MINUTES

Members Present

Mayor J. Michael Ross
Councilman E. Baughman
Councilman L. Griffin
Councilman M. Gordge

Staff Present

Brian Cook, Town Admin
Jim Meggs, Attorney
Melissa Cowan, Town Clerk
Michael Criss, Consultant

Councilmembers Absence

Councilman B. Franklin, Excused

I. **CALL TO ORDER**

A. ROLL CALL

Mayor Ross called the meeting to order at 7:03 PM. Four Council members were present, constituting a quorum.

B. NOTIFICATION AND POSTING OF THE MEETING AGENDA

Town Clerk confirmed the agenda was properly posted and the media notified.

C. APPROVAL OF THE AGENDA

Councilman Baughman made a Motion to approve the agenda. The Motion was seconded by Council Griffin. **All in favor; 4-0**

D. PLEDGE OF ALLEGIANCE

Boy Scout Mount of troupe 224 led the Pledge of Allegiance.

E. INVOCATION

The evening's invocation was given by Councilman Griffin.

F. COUNCIL MEMBERS' REMARKS

Councilman Baughman made the following remarks:

- He thanked the Blythewood Chamber of Commerce for organizing the ribbon cutting ceremony at Founders Federal Credit Union. He said that he was very proud to represent the Town of Blythewood.

- He gave everyone a safety warning concerning pickup trucks being stolen within the last couple of weeks. He asked everyone to keep their vehicles locked and don't leave anything of value in it.

Councilman Gordge made the following remarks:

- He stated that he had a couple of quick updates and would be talking more later on some of them.
- The repair work to our bridge has been delayed a couple of weeks. We expect work to start in early April but do not have a firm date yet. We will keep you posted through the Town's News letter.
- A representative from Dominion (Now own SCANA/SCE&G) spoke extensively at the March CMCOG meeting about the V.C. Summer plant and the implications of the financial settlement. In his opinion, the plant would never be finished due to the debt liability.
- Bravo Blythewood is still asking for residents to complete their survey (newsletter for link) about preferences for events at Doko Meadow. So far, Saturday seems the favorite time and \$10 a ticket was a good ticket price.
- He mentioned that he participated in the Longleaf Careers Fair on last Thursday which was a lot of fun. He said that he spoke with some budding Mayors and community leaders.

Councilman Griffin made the following remarks:

- He wished everyone a Happy Easter

G. MAYOR'S REMARKS

- He mentioned an event that was held at Sweet Peas Ice Cream Parlor. He said that the event featured a few local artists.
- Mayor Ross recognized Barbara Ball for winning the Jay Bender Award for Assertive Journalism.

H. APPROVAL OF MINUTES

Councilman Baughman made a Motion to approve the minutes of February 26, 2018. The Motion was seconded by Councilman Griffin. **All in favor; 4-0**

II. PUBLIC HEARING REGARDING LANDSCAPING AND BUFFER YARD REQUIREMENTS

Mayor Ross opened Public Hearing at 7:17pm.

Shane Alford spoke on behalf of Essex Homes. He said "The concerns are specifically over and beyond the intent of the repeal Ordinance 2018.002. What implications that repeal will have on a broader scope to projects that are already permitted and already in existence." He asked Council to give consideration to the unintended consequences of such an appeal.

Earl McCloud Executive Building Industry Association, spoke against proposed Ordinance 2018.002. He asked Council to give serious consideration to changing the rules as they would effect existing projects.

Doug Shea of 392 Summersweet Court spoke in support of proposed Ordinance 2018.002.

Mayor Ross Closed the Public Hearing at 7:31pm.

III. DISCUSSION ITEMS

A. Review of Retreat, Summary and Take-Away Items

Councilman Gordge stated that the retreat was a very successful event, and that it gets better every year.

Mayor Ross thanked each of the vendors for providing lunch.

B. Selection of Master Plan Priorities

a. Road Improvements

Councilman Gordge stated that there are currently two key elements to road improvements and they are bridge repairs and the Blythewood road widening which is one of the projects through the Richland Penny Tax Program. Phase 2 of the penny money will be used to improve McNulty, Blythewood Road east, traffic circle at Creech Road and picking up from Syrup Mill going down to Fulmer Road.

b. Multi – use trails

Mayor Ross stated that the Planning Commission has a subcommittee heading this item.

c. E-market the Town's amenities/services and development opportunities

Economic Development Consultant, Ed Parler will assist with this item.

d. Annexation opportunity for homeowners outside Town limits

Councilman Griffin and Administrator Brian Cook will head this project.

e. Expanding ball field/recreational facilities

Councilman Franklin and Administrator Cook will head this project.

f. On-going development of Doko Meadows Park

Councilman Baughman and the Park Foundation will head this project.

IV. ACTION ITEMS

Citizens Testimony Regarding Action Items (As item is presented)

A. Approval of priorities for Phase 2 Road Improvement Program

Mayor Ross said that he would recommend prioritizing item A as the next thing to be submitted to the Penny Sales Tax Committee for Phase 2.

- a. McNulty from Main to Blythewood Road – improve to a 3 lane section (2 lanes with center turn lane)

Councilman Baughman made a Motion prioritize item a. The Motion was seconded by Councilman Gordge. All in favor; 4-0

- b. Blythewood Road. From I-77 to Main – widen to 5 lanes (4 lanes with center turn lane)
- c. Traffic Circle at Blythewood Road. And Creech Road
- d. Creech Road. Extension to Main – new 3 lane connection (2 lanes with center turn lane)
- e. Blythewood Road. From Syrup Mill to Fulmer – Improve to 3 lanes (2 lanes with center turn lane)

Councilman Baughman made a Motion to postpone items B-E to a date to be determined. The Motion was seconded by Councilman Griffin. All in favor; 4-0

V. OPEN CITIZENS COMMENT

Ann Leavitt spoke on the roundabout
Ann Childers spoke on the roundabout
Kambrell Garvin candidate for South Carolina House of Representatives District 77 announced the start of his election campaign.

VI. ADJOURNMENT

Councilman Baughman made a Motion to adjourn the meeting at 8:13pm. The Motion was seconded by Councilman Gordge. All in favor; 4-0

Respectfully submitted,

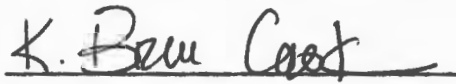

Melissa Cowan, Town Clerk

FORTH priority: to widen and improve Blythewood Road from Syrup Mill to Fulmer Road (e.g. addition of bike lanes, etc.)

ATTEST:

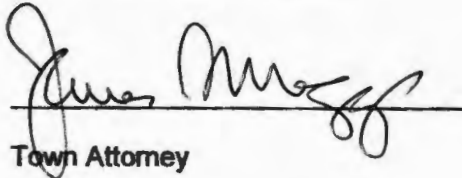


Mayor



Town Administrator

APPROVED AS TO FORM:



Town Attorney



TRANSPORTATION
PROGRAM

February 12, 2019

Attention To: David Beaty, PE

RC Project Name: 271 Atlas Road Widening

Utility Agreement No.: 271-02

Utility Owner: SCE&G Distribution
Connie Beall
220 Operation Way (MC:J29)
Cayce, SC 29033

Description: Request for Utility Agreement Approval and the creation of a Purchase Order
See Attached Utility Agreement and supportive documentation.

The Atlas Road Widening project requires the relocation of SCE&G Distribution facilities throughout the project. The Utility Agreement that has been submitted to the PDT is being submitted for approval prior to award of the project due to the SCE&G material order lead time of 6 months.

Rebecca Connelly
PDT Utility Coordinator

Feb. 12, 2019
Date

UTILITY AGREEMENT

Richland County Project No. 271 Route (or Road No.) Atlas Road Widening

This Agreement made this _____ day of _____, 20____, by and between Richland County, hereinafter called "County" and the SCE&G hereinafter called "Company".

W I T N E S S E T H:

1. It is mutually agreed by and between the parties hereto that the Company shall perform or cause to be performed, the following work to its utility property facilities as shown on the attached plans and estimate:
General description: SCE&G will relocate electric distribution facilities in conflict with this project. See attached SCE&G relocation drawings for details.

2. The Company hereby agrees to relocate its utility facilities in conflict with highway construction in accordance with the provisions set forth in the Federal Highway Administration's FAPG 23 CFR 645A; and also in accordance with Richland County's Utility Coordination Manual in the estimated amount of

\$ 3,156,285.00

Richland County Share \$100% Utility Share \$0%

(a) The Company (does, does not) have the right of occupancy in its existing location by reason of holding the fee, as easement or other real property interest, the damaging or taking of which is compensable in eminent domain. Upon completion of the relocation and submittal of the final invoice as detailed in Number 9, the Company must provide documentation that the portion of the utility easement acquired by the County on behalf of the County as part of the new right of way acquisition for the construction project has been relinquished. The Company must also supply an affidavit stating all legal documents were filed at the County courthouse where the easement is filed. If the Company does not have the right of occupancy in its existing location by reason of holding the fee, an easement or other real property interest, (Explain) _____

Easement for this project is filed in Richland
(County)
Columbia, SC
(City and State)

(b) This section of line (for purposes of establishing right-of-way priorities only) has been in service for approximately 30 years or more.

(c) Such work as is necessary to relocate, alter or maintain the facility will be done in such a manner that it will not in any way interfere with or endanger the safety of the general public in their use of the roads as a highway. Traffic control and signing will be coordinated with the County's contractor and will be in accordance with "The South Carolina Manual on Uniform Traffic Control Devices for Streets and Highways" (SCMUTCD).

3. The Company shall begin said work promptly upon notification in writing by the County and dispatch to its completion as promptly as is practicable. The Company shall provide advance notice to the County of the date on which work is expected to begin.

4. The Company will perform the work provided for in this agreement by the method checked below, in accordance with the provisions of Sec.645.115 of FAPG 23 CFR 645A:

BY COMPANY'S REGULAR FORCES

BY CONTRACT: (State one of recognized reasons for necessity of performing work in this manner)

The Company, therefore, subject to prior approval, proposes to contract a portion of or all of the work covered by this agreement. The items of work to be accomplished by contract are noted in the estimate. Where the Company elects to solicit competitive bids from a list of qualified contractors rather than through advertising in a publication, the names and addresses of those contractors so circularized shall be noted on the estimate or furnished to the County in advance of the Company's solicitation of bids.

BY EXISTING CONTINUING CONTRACT: Subject to prior approval and inspection by the County and the prior approval of the contract method by the Federal Highway Administration, the Company proposes to use an existing continuing contract under which it is demonstrated that such work is regularly performed for the Company under such contracts at reasonable costs. The provisions relating to contracts under this paragraph also apply to Contract Engineering.

5. The County will reimburse the utility company for costs incurred in accordance with the agreement as shown and on accurate records supporting all expenditures incurred on account of said work. The method of developing the relocation costs shall be one of the following alternatives.

(a) Actual and related direct costs accumulated in accordance with a work order accounting procedure prescribed by the applicable Federal or State regulatory body.

(b) Unit costs, such as broad gauge units of property, as used in own operations. (This method must have prior approval.)

(c) Lump Sum
The County may not pay for any item of work not provided for in the plan or in the cost estimate except as modified by the County Engineer followed by written notification to the County.

6. All work performed by the Company pursuant hereto, shall be performed according to the plans and estimates approved by the County.

7. Attached hereto, and by reference made a part hereof, is one copy of plans and estimates of the work to be performed by the Company showing the existing facilities, permanent changes to be made therein, and the stages by which these changes are to be accomplished.

8. Credits for accrued depreciation (expired service life) and/or betterment of the facility have been allowed as outlined in Sec.645.117(h) of FAPG 23 CFR 645A.

9. Periodic progress billings of incurred costs may be made by the Company to the County not to exceed monthly intervals and amounting to at least \$2500.00; however, total progress billing payments shall not exceed the approved nonbetterment estimated amount.

Upon completion of the work and no later than six months thereafter, the Company shall, at the earliest date practicable, furnish the County with five (5) copies of its final and complete billing of all costs incurred in connection with the work performed hereunder, such statement to follow as closely as possible the order of the

items contained in the estimate. The totals for labor, overhead construction cost, travel expense, transportation, equipment, material and supplies, handling cost and other services shall be shown in such a manner as will permit ready comparison with the approved plans and estimates. The utility company shall provide the documents related to relinquishing their rights to the easement acquired by the County. Items of materials shall be itemized where they represent major components.

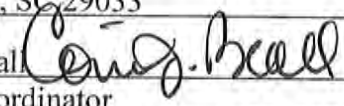
Credit shall be given for usable materials recovered from permanent or temporary installations. The final billing shall show the description and site of the project, the Federal Aid Project Number, the date on which the first work was performed; or, if preliminary engineering or right-of-way items are involved, the date on which the earliest item of billed expense was incurred, the completion date and the location where the records and accounts billed can be audited. The Company shall make adequate reference in the billing to its records, accounts and other documents. Contractors and any subcontractors are to maintain all books, documents, papers, accounting records and other evidence pertaining to costs incurred and to make such materials available at their respective offices at all reasonable times during the contract for inspection by the County, Federal Highway Administration or any authorized representatives of the Federal Government and copies thereof shall be furnished if requested.

Final billings of incurred costs submitted by the Company shall carry a statement certifying that all items claimed have been reviewed and are in conformity with the provisions of the agreement; that credits have been given for all salvaged materials as required, and that all contractor's bills incurred have been paid in full. This statement shall be signed by an authorized representative of the Company.

In the event a final and complete billing has not been received by the County prior to six months following the completion of work and the Company has not, during that period, demonstrated to the County's satisfaction a hardship in completing that billing, the County may, in its sole discretion, consider the last payment made to be the final payment due under this Agreement. If the utility company is aware they will be unable to provide close-out and final invoicing within the six month period following completion, they may provide a letter requesting additional time to the County for consideration.

- 10. The County, and the Federal Highway Administration shall have the right to inspect recovered materials from the permanent facility prior to disposal by sale or scrap. This requirement will be satisfied by the Company giving notice to the County of the time and place the materials will be available for inspection. This notice is the responsibility of the Company and it may be held accountable for full value of materials disposed of without notice. The County, and the Federal Highway Administration shall have the right to inspect all recovered materials, which are not reusable by the Company, for temporary use. The Company shall furnish a listing on final billings of major items not eligible for salvage credit, and reasons therefor.
- 11. The Company will abide by the contract cost principles as set forth in FAPG 23 CFR 645A.
- 12. The Company will not participate directly or indirectly in any practice which subjects persons to discrimination because of their race, color, religion, sex or national origin.

INSTRUCTIONS: Submit two original copies of the agreement form and six prints of drawings (or one reproducible print) showing present location and proposed location of poles or lines with reference to highway survey stations and centerline.

COMPANY: SCE&G
 ADDRESS 220 Operation Way (MC: J29)
Cayce, SC 29033
 BY: Connie J. Beal 
 TITLE: Highway Coordinator

RICHLAND COUNTY

BY: _____

South Carolina Electric and Gas Company

OWIP Invoice / Estimate

Report Date: 06/14/2018

WO #: 027130

Project Title: ATLAS RD WIDENING, PENNY TAX PROJECT

Work Request #: 197774

Work Area: METRO COLUMBIA CREW HDQR

Designer: RAINES, RUSSELL

Tax District: 401CCO COLUMBIA

Coordinator: RAINES, RUSSELL

Schedule Start:

Drawing #: 81994

Schedule Comp:

Nature of Request: RELOCATION OF OVERHEAD LINES ALONG ATLAS RD, FOR RICHLAND COUNTY PENNY TAX PROJECT

<u>Description</u>	<u>Estimated Cost</u>
Material Cost	\$387,800
Line Salvage	\$0
Stores and Handling	\$62,732
Total Construction Labor	\$1,585,780
Total Labor Overheads	\$0
Total Transportation Charges	\$0
Administrative Overheads	\$372,473
R/W Clearing	\$0
Associated Cost	
ROW TREE TRIMMING/CLEARING	\$300,000
TRAFFIC CONTROL / FLAGMEN	\$100,000
SURVEYING TO STAKE POLE LOCATIONS	\$100,000
STEEL POLE PT 40.3	\$27,500
STEEL POLE PT 40.4	\$27,500
STEEL POLE PT 40	\$27,500
STEEL POLE PT 39	\$27,500
STEEL POLE PT 38	\$27,500
STEEL POLE PT 37	\$27,500
STEEL POLE PT 33	\$27,500
STEEL POLE PT 29	\$27,500
STEEL POLE PT 28	\$27,500
Sub Total	\$3,156,285
Customer Credit / Betterment	\$0
Total Cost	\$3,156,285

Approvals _____ **Date:** _____

Note: A Copy Of This Document Must Accompany Customers Remittance.

Estimate Date - 2018-06-12



TRANSPORTATION PROGRAM

Date: 2/13/19

To: Dr. John M. Thompson, Ph.D., MBA, CPM
Director of Transportation

From: David Beaty, PE
Program Manager

RE: Shop Road Widening – Recommended Revision to Terminate Improvements at Mauney Drive

Recommendation: Revise the end termini of the project from S.Beltline Boulevard to Mauney Drive.

Background

The Shop Road Widening project as referenced in the 2012 Penny Sales Tax Referendum proposes to widen the existing corridor from George Rogers Boulevard to S. Beltline Boulevard to a 5-lane section for a total distance of approximately 2.50 miles. The referendum included an allocation of \$33.1 million for this work. The current design for this project proposes a 5-lane section with curb and gutter and offset shared-use paths on each side of the road within these limits for an estimated cost of \$61.5 million (2018 Q4 estimate).

Shop Road is a 5-lane section at the intersection with Mauney Drive as it exists today and this section continues to the intersection with S. Beltline Boulevard. The existing roadway section east of Mauney Drive currently maintains the same traffic capacity as proposed by the plans to widen the road in this section. Therefore, no traffic operational improvements would be realized by widening past Mauney Drive. Also, Norfolk-Southern railroad owns a triple rail crossing between Mauney Drive and S. Beltline Boulevard. Improvements to the roadway at these crossings would require updating the crossings to current standards, including warning devices and crossing arms (which do not exist today). This recommended revision would reduce the overall project length by approximately 0.22 miles. See Exhibits A & B, attached, detailing the currently proposed project limits and the area of revision between Mauney Drive and S. Beltline Boulevard.

A analysis of this revised termini was conducted to evaluate costs associated with construction, rights of way acquisitions, utility conflicts and relocations and costs associated with the railroad crossing. This analysis has concluded that a savings of approximately \$3.1 million could be realized by revising the design to incorporate this change in termini – see Table 1, below for a break-down of associated costs.

Table 1: Approximate Cost Savings by service-type or issue

Approx. Cost Savings	
Construction-Related Costs	\$ 1,450,000.00
Rights of Way Services & Acquisitions	\$ 350,000.00
Utility Relocation Costs	\$ 300,000.00
Railroad-Related Costs	\$ 1,000,000.00
Total	\$ 3,100,000.00

Exhibit A: Detail of Shop Road Widening Project Limits

Exhibit B: Detail of Shop Road between Mauney Drive and S. Beltline Boulevard

Exhibit A: Detail of Shop Rd Widening Project Limits

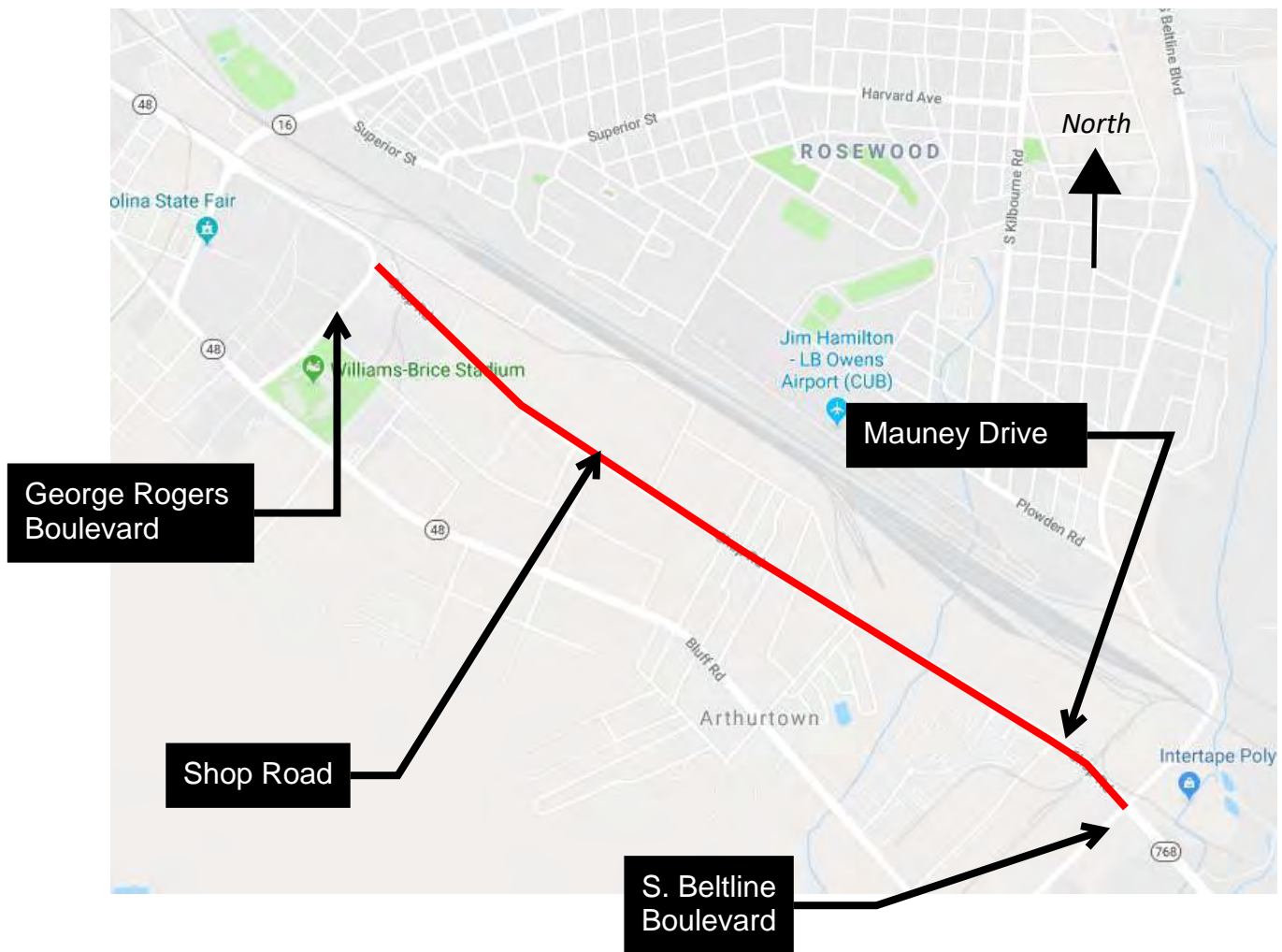
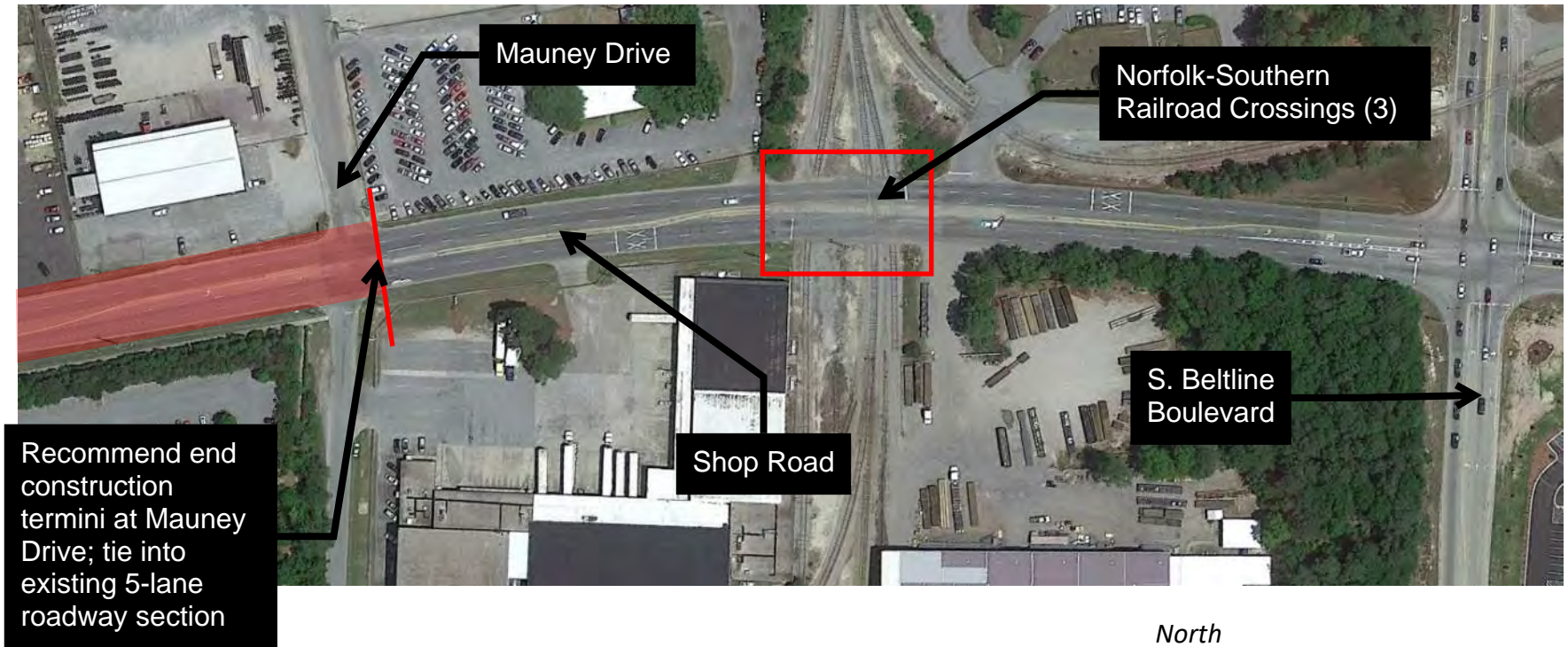


Exhibit B: Detail of Shop Rd between Mauney Dr. and S.Beltline Blvd.



North



Service Order
For
On Call Engineering Services Agreement

SERVICE ORDER NO. C&D #9

Date: February 13, 2019

This Service Order No. C&D #9 is issued by Richland County, South Carolina (the “County”), to Cox and Dinkins, Inc. (the “Consultant”) pursuant to that Agreement dated February 11, 2015 between the County and the Consultant called “On Call Engineering Services Agreement Related to the Richland County, South Carolina Sales Tax Public Transportation Improvement Plan” (the “Agreement”).

This Service Order, together with the Agreement, form a Service Agreement. A Service Agreement represents the entire and integrated agreement between the parties and supersedes prior negotiations, representations, or agreements, either written or oral. A Service Agreement may be amended or modified only by a Change Order or Change Directive as provided for in the Agreement.

I. Scope of Services.

A. Unless otherwise provided in an exhibit to this Service Order, this Service Order and the Service Agreement are based on the information set forth below:

See Exhibit A – Scope of Services

B. Unless otherwise provided in an exhibit to this Service Order, the Consultant’s Services to be provided pursuant to this Service Order are:

See Exhibit A – Scope of Services

C. Unless otherwise provided in an exhibit to this Service Order, the County's anticipated dates for commencement of the Services and Completion of the Services are set forth below:

1. Commencement Date: March 8, 2019
2. Completion Date: *See Exhibit A – Scope of Services - Schedule*

D. Key personnel assigned by Consultant to this Service Scope of Work:

1. Gene Dinkins, PE, PLS (*Principal-in-Charge*)
2. McTilden “Mac” Atkins, III, PE (*Project Manager*)

II. Insurance

The Consultant shall maintain insurance as set forth in the Agreement. If the Consultant is required to maintain insurance exceeding the requirements set forth in the Agreement, those additional requirements are as follows:

N/A

III. Safety and Warranty

It is understood that the Consultant does not have a Safety Supervisor or anyone in a similar position on staff and is not responsible in any way for job site safety or security. However to the extent that the Consultant does have employees or representatives on site, these persons will respect the safety of the public.

It is understood that the Consultant cannot give a warranty on professional services. The Consultant provides professional services (not goods) and shall only be held to a negligence – based standard of care that is guided by what a reasonable design professional would do under similar conditions in the same location and at the same time.

IV. Owner’s Responsibilities.

In addition to those responsibilities the County may have as stated in the Agreement, the County in connection with this Service Order only shall:

N/A

V. Consultant’s Compensation.

A. The Consultant shall be compensated for Services provided under this Service Order as follows:

<i>Lump Sum</i>	\$	488,498.00
<i>Approved Direct Expenses</i>	\$	<u>37,365.00</u>
		525,863.00
 <i>Contingency – Not to Exceed¹</i>	 \$	 48,849.80

¹ Requires approval from Richland County to authorize contingency

B. Additional Services. Unless otherwise provided in an exhibit to this Service Order, any Additional Services by the Consultant shall be paid as Additional Services as provided in the Agreement.

VI. Additional Exhibits.

The following exhibits and/or attachments are incorporated herein by reference thereto:

See Exhibit A – Scope of Services

VII. Execution of Service Agreement

The Execution of this Service Order by the County below constitutes a Service Order to the Consultant. The execution of this Service Order by the Consultant creates the Service Agreement.

NOW, THEREFORE, in consideration of the foregoing, the sufficiency of which is hereby acknowledged by the parties, this Service Agreement is entered into Under Seal as of the Effective Date of _____, 2019.

WITNESS:

RICHLAND COUNTY, SOUTH CAROLINA

By: _____ (L.S.)

Its: _____

Date: _____

CONSULTANT:

COX AND DINKINS, INC.

WITNESS:



By:  (L.S.)

Its: Project Manager

Date: 2/15/19

EXHIBIT A: SCOPE OF SERVICES

ATTACHMENT "A"
SCOPE OF SERVICES & SCHEDULE
CLEMSON RD / SPARKLEBERRY LN
INTERSECTION IMPROVEMENT PROJECT

Introduction

Cox and Dinkins, Inc. (CONSULTANT) has been authorized by Richland County (COUNTY) to provide construction-phase design engineering services for the proposed Clemson Rd / Sparkleberry Ln Intersection Improvement Project (PROJECT) in Richland County, South Carolina, based on the services and design reflected in the Final Right-of-Way Plans.

Summary of Anticipated Services - An outline of the services anticipated for this project is shown below.

- Task 1 – Project Management
- Task 2 – Environmental / Public Meeting – No services included
- Task 3 – Field Surveys
- Task 4 – Roadway Design
- Task 5 – Pavement Marking & Signing Plans
- Task 6 – Traffic Signal Design
- Task 7 – Lighting Plan
- Task 8 – Work Zone Traffic Control Design
- Task 9 – Stormwater Management / Hydraulic Design
- Task 10 – Sediment & Erosion Control / NPDES Permitting
- Task 11 – Geotechnical Explorations and Engineering Services
- Task 12 – Roadway Structures Design and Plans
- Task 13 – Subsurface Utilities Engineering (SUE)
- Task 14 - Utility Coordination Assistance –
- Task 15 – Construction Phase Assistance –

Quality Control

The CONSULTANT shall implement all necessary quality control measures to produce plans and reports that conform to COUNTY guidelines and standards. Prior to submittal to the COUNTY, all plans and reports shall be thoroughly reviewed for completeness, accuracy, correctness, and consistency. Subconsultants for this project will be required to implement and maintain a stringent quality control program as well. The COUNTY reserves the right to request

QA/QC documents (red-lines, checklists, etc) from the CONSULTANT with project deliverables.

Task 1

PROJECT MANAGEMENT

The CONSULTANT shall institute a program for conformance with COUNTY requirements for monitoring and controlling project engineering budget, schedule and invoicing procedures. The CONSULTANT's subconsultants shall be included in this program. Proposed dates of submittals, completion of tasks, and final completion of pre-construction services as noted in this agreement will be negotiated with the COUNTY. Included in management of the project will be:

- ◆ Project meetings between the COUNTY, South Carolina Department of Transportation (DEPARTMENT), and CONSULTANT for clarification of scope, discussion of concepts, review of submittals, etc. at the discretion of the COUNTY.
- ◆ The CONSULTANT will prepare meeting agenda and meeting materials as well as record the minutes of each meeting in which it participates and distribute to the appropriate COUNTY personnel. Meeting agenda shall be prepared and submitted to COUNTY within two (2) business days prior to a scheduled meeting. Meeting minutes shall be prepared and submitted to the COUNTY within three (3) business days after a scheduled meeting.
- ◆ Prepare monthly invoices, status reports, and schedule updates. Assume an 8 month design schedule which will impact the duration of preparing invoices, status reports, and schedule updates.
- ◆ The CONSULTANT will provide coordination with its SUB-CONSULTANTS during the execution of their work. Assume a 4 month design schedule specific to sub-consultant coordination.
- ◆ The CONSULTANT will include the COUNTY in any discussions concerning the project prior to submittal of deliverables if that process has the advantage of expediting the completion of any task of the project.

The CONSULTANT will attend meetings with the COUNTY and stakeholders from various organizations affected by this project in order to incorporate the needs and desires of these organizations into the decision-making process. It is assumed that the CONSULTANT will attend eight (8) project meetings and up to two (2), additional review coordination meetings with the DEPARTMENT and the COUNTY. The CONSULTANT will be in attendance at these meetings and will prepare all necessary display materials and meeting agendas & minutes.

TASK 2

ENVIRONMENTAL / PUBLIC MEETING

As there are no wetlands adjacent to the project site, nor those that would be directly affected by the project, no wetland permitting is assumed necessary. No environmental documentation or reports are assumed for this scope of services. Two public meetings were held for this project during the preliminary design phase. No additional public involvement meetings are anticipated or associated with this service order modification.

Task 3

FIELD SURVEYS

No additional field survey services are assumed to be conducted as part of this service order modification (other than those stated below). The CONSULTANT will be responsible for obtaining and performing any supplemental surveys necessary to facilitate design, permitting and development of plans for the project, as covered under the original contract (Phase 1).

The CONSULTANT will stake and obtain boring elevations for all final geotechnical borings performed on the project by the CONSULTANT. The CONSULTANT shall assume 18 borings to be surveyed. Survey of preliminary geotechnical borings were covered under the original contract.

Task 4

ROADWAY DESIGN

The Roadway Design services included in this service order modification are specific to those activities associated with final construction design and plan development and specific design evaluations and updates as described below. The previous scope of work included those services necessary for the development of Final Right-of-Way design and plans for approval by DEPARTMENT.

Design Evaluations & Updates The CONSULTANT will evaluate and update the roadway design and plans specific to the following;

1. **Tract 33 New Location Access Drive** – the design should be evaluated to provide a two-way ingress/egress. Included is the evaluation of options to allow full-access to Sparkleberry Crossing Road; current sight distance obstructions (specifically fencing and vegetation around County-owned detention pond on tract 54) limit intersection sight distance for this proposed access. The CONSULTANT will evaluate the feasible options to provide full access and provide to the COUNTY prior to updating the design and plans. Final design for this access will include profile and cross section sheets. It is assumed that this access drive will be constructed by permission and no new right of way will be acquired.
2. **Clemson Road Termini** – The CONSULTANT will conduct all necessary design and plan updates associated with extending the project approximately 400' east along Clemson Rd. from the existing termini at approximate station 32+50 to the northeast, terminating just before Chimney Ridge Road, to tie to the proposed beginning termini as reflected in the Clemson Road Widening Final Construction Plans. The proposed work included in this service order modification assumes a mill and resurfacing of the existing pavement section, addition of new curb and gutter and offset, shared-use pathways on both sides of the road, including any necessary closed-system drainage improvements. The work covered under this specific scope item was not assumed under the original contract scope of services.

The design evaluations and plan updates as described above include all associated roadway design and plan update efforts and drainage system evaluations, updates or improvements, including hydraulic design modeling and/or updates to the previously developed draft Stormwater Management Report.

The evaluations and updates will be designed to meet all necessary criteria as defined in the original contract scope of services. Should differing, or reduced criteria be recommended for any of the design evaluation and updates stated above, the CONSULTANT should notify the COUNTY prior to moving forward with associated design services.

It is assumed that the design evaluation and plan updates will be reflected in the preliminary construction plans (95% complete) as detailed in scope of work below.

Final Roadway Design and Plans

Roadway Construction Plans – The construction plans will be a continuation of the Right-of-Way Plans, including those updates as detailed above. Original Right-of-Way Plans will be retained by the CONSULTANT after appropriate COUNTY reviews and signatures and then developed into construction plans.

Plan and profile sheets will show information necessary to permit construction stakeout and to indicate and delineate details necessary for construction.

Construction plans shall incorporate all items presented in the Roadway Construction Plans section of the DEPARTMENT's *Road Design Reference Material For Consultant Prepared Plans* dated June 2010.

The CONSULTANT will attend the Final Roadway Plans Design Field Review with the COUNTY to review the project design in the field. The CONSULTANT will prepare meeting minutes / summary of discussions from the design field review. The final design field review will be scheduled approximately 2 weeks after submittal of the preliminary construction plans (COUNTY to coordinate field review). This review may be held in the offices of the Richland Penny Program rather than in the field. However, for fee estimating purposes, the CONSULTANT should assume a meeting on-site.

The CONSULTANT shall submit set of preliminary construction plans (assumed 95% complete) to the COUNTY for review and comment. Upon receipt of the revised preliminary construction plans from the CONSULTANT, the COUNTY shall submit the revised Preliminary Construction Plans to the DEPARTMENT for review and comment. Following the review of the preliminary construction plans by the DEPARTMENT, the CONSULTANT shall revise the plans and submit final construction plans to The COUNTY for review and comment. Upon receipt of the revised final construction plans from the CONSULTANT, the COUNTY shall submit the revised final construction plans to the DEPARTMENT for review and comment. Following the review of the final construction plans by the DEPARTMENT, the CONSULTANT shall finalize the plans and submit the released for construction (RFC) plans (signed and sealed by a Professional Engineer licensed in the state of South Carolina).

It is anticipated that the COUNTY and the DEPARTMENT will each perform one (1) review and comment of the preliminary construction plans and one (1) review and comment of the final construction plans. The CONSULTANT will be responsible for updating all plan deliverables per COUNTY and DEPARTMENT reviews. The CONSULTANT shall also be responsible for providing responses to all COUNTY and DEPARTMENT comments documented within typical comment matrices.

The CONSULTANT will provide two (2) half-size plan sets for the submittal of preliminary construction plans (at each review) along with a PDF of the plan set. The CONSULTANT will provide one (1) full-size and one (1) half-size plan set for the submittal of Final Construction plans along with a PDF of the plan set.

The CONSULTANT will provide final construction CADD files to the COUNTY with submittal of the Final Construction plans.

The Preliminary Construction cost estimate will be updated by the CONSULTANT and submitted with the Preliminary Construction Plans for use by the COUNTY.

On or before the contract completion date, the CONSULTANT will deliver to the COUNTY one complete set of Final Construction Plans, an Engineer's Estimate, and "Project Specific" Special Provisions. See Project Special Provisions and Engineer's Estimate for the description of the Engineer's Estimate and "Project Specific" Special Provisions.

Project Special Provisions and Engineer's Estimate of Probable Cost – The CONSULTANT will prepare all "Project Specific" Special Provisions and include them in the format compatible with the DEPARTMENT Construction Administration Section. The CONSULTANT will work closely with COUNTY personnel for the COUNTY's development of the construction document package.

Also, utilizing recent bid data from similar projects in the area, the CONSULTANT will prepare an Engineer's Estimate of Probable Cost for construction of this project. The estimates will be based on the final summary of quantities and will be utilized in the final bid analysis and award.

Determination of the bid form for the construction contract will be determined from the Engineer's Estimate of Probable Cost and an estimate of project cost as developed by the COUNTY. Upon submittal of the Engineer's Estimate of Probable Cost, it is assumed the CONSULTANT will attend one (1) meeting to discuss quantities and unit prices for development of the final contract bid form.

For this task and all other tasks contained in this scope, the CONSULTANT will utilize the DEPARTMENT standard drawings, specifications, and design manuals that are current as of the first issuance of the task order scope by the COUNTY to the CONSULTANT.

Task 5

PAVEMENT MARKING/SIGNING PLANS

Final pavement marking/signing plans for the project will be prepared at a scale of 1"=50' unless otherwise agreed upon. The plans will consist of an itemized listing of estimated quantities; typicals for installation, details showing lane lines, edge lines, stop bars, symbol and word messages and any other appropriate markings and sign designation numbers and locations. The plans will include dimensions sufficient for field layout. The *Manual on Uniform Traffic Control Devices (MUTCD): 2009 Edition* and DEPARTMENT details will be incorporated into the plans.

Pavement Marking / Signing plans will be included in the preliminary construction plan submittal for review and comments by the COUNTY and DEPARTMENT prior to drafting of the final details.

Task 6

TRAFFIC SIGNAL DESIGN AND PLANS

The CONSULTANT shall prepare traffic signal design and plans at a scale of 1"=30' as required for the project. Communication plans including fiber optic cable routing and radio communication shall be prepared at a scale of 1"=50' scale as required for the project. Coordination traffic signal system timing sheets shall be provided for up to 6 time of day/day of week timing plans. Traffic signal plans shall conform to the *Manual on Uniform Traffic Control Devices (MUTCD): 2009 Edition*, DEPARTMENT Standard Drawings, and SCDOT Traffic Signal Design Guidelines: 2009 edition. The signal plans shall show the placement of mast arms, steel strain pole supports, location of signal heads, location of camera detectors and detection zones, the lane configuration, signing related to the signals, pedestrian appurtenances, as applicable and other details pertinent to the layout of the signal. The plans shall also show any necessary adjustments to the operating signal sequence, the signal timing and existing signal equipment. The CONSULTANT shall prepare Special Provisions for Traffic Signal Installation based on current DEPARTMENT guidelines.

The CONSULTANT will conduct 14-hour turning movement counts (6AM to 8PM) at the following intersections to be utilized in the development of coordinated traffic signal system timing:

- Clemson Road and I-20 WB Ramp
- Clemson Road and Clemson Frontage Road/Corporate Park Drive
- Clemson Road and Sparkleberry Road
- Clemson Road and Sparkleberry Crossing Road
- Sparkleberry Lane and Sparkleberry Crossing Road
- Sparkleberry Lane and Mallet Hill Road

The COUNTY will provide existing timing and plans to the CONSULTANT, as available.

The scope of services stated above will include the traffic signal design and plans to be developed for the following intersections;

- Clemson Rd / Corporate Park Dr / Clemson Frontage Rd – *full re-build – Steel Strain Pole/ Span Wire Design*
- Clemson Rd / Sparkleberry Ln – *full re-build – Mast Arm Design*

- Clemson Rd / Sparkleberry Crossing – *new signal installation – Mast Arm Design*
- Sparkleberry Ln / Sparkleberry Crossing – *new signal installation – Steel Strain Pole/ Span Wire Design*
- Sparkleberry Ln / Mallet Hill Rd – *full re-build – Steel Strain Pole/ Span Wire Design*

Traffic signal phasing, plans and coordinated signal timing may be modified up to (4) times for the construction phasing utilizing the traffic count data collected in the services above. Temporary traffic signal design will utilize steel strain pole/span wire design at the intersections where the final signal design incorporates mast arm structures.

Task 7

LIGHTING PLAN

A lighting plan will be provided and will be prepared at a scale of 1"=50' unless otherwise agreed upon. The lighting plan will also include a photometric analysis. The plans will include dimensions sufficient for field layout. The approaches to the diverging intersections shall be illuminated as required by guidelines provided by the *Design Guide for Roundabout Lighting, NCHRP Report 672 Roundabouts: An Information Guide – 2nd edition*, as well as the limits of the diverging intersection from the crossover at Sparkleberry Lane to the crossover at Sparkleberry Crossing Drive per the guidelines provided by *AASHTO Roadway Lighting Design Guide (latest edition)*. No lighting is proposed along Sparkleberry Lane and Sparkleberry Crossing Road (or other side roads and intersections affected by this scope of work), except as necessary per the guidelines provided above.

The lighting plan will be provided with the preliminary construction plans, for review by the COUNTY and DEPARTMENT with comments addressed and updated with the final construction plans.

Task 8

WORK ZONE TRAFFIC CONTROL

The design and preparation of one set of Work Zone Traffic Control plans will be accomplished for the project and included with the respective construction plans. The plans will include a description of the sequential steps to be followed in implementing the plans, and will be developed at a scale of 1"= 50', unless otherwise agreed upon. The traffic control plans will include lane closures and traffic shifts, traffic control devices, temporary lane markings, and construction signing and sequencing notes. The plans will identify lane widths, transition taper widths, and any geometry necessary to define temporary roadway alignments. Also, the plans

will address the type of surface to be used for all temporary roadways. Standard traffic control details will be incorporated into the plans for most work activities, but detailed staging plans will be required where impacts upon the normal traffic flow are significant.

Preliminary traffic control plans will be submitted in conjunction with the preliminary construction plans, and the final signed and sealed traffic control plans along with final quantities will be submitted with the final roadway construction plans.

Task 9

STORMWATER MANAGEMENT/HYDRAULIC DESIGN

The Stormwater Management services included in this service order modification are specific to those drainage design activities associated with final construction design and plan development and Final Stormwater Management Report. The previous scope of work included the drainage design services necessary for the development of Final Right-of-Way design and plans for approval by DEPARTMENT.

The design for final construction plans will include updating the design as necessary to account for any changes in the proposed road design including any necessary changes as a result of modifications to curb grades. It is assumed that there will not be any adjustments to the profiles or alignments of Driveway 1, Driveway 2, Round 1, raised concrete medians as shown in the right-of-way plans.

Task 10

SEDIMENT AND EROSION CONTROL/NPDES PERMITTING

Sediment and Erosion Control – The intersection improvement project included in this scope of work will include the development of Sediment and Erosion Control Plans as well as the preparation of Supporting Documentation for the Land Disturbance Permit Application.

The erosion control plans will be prepared on replications of the roadway/drainage plan sheets and at the same plan scale, unless otherwise agreed upon. The erosion control plans will reflect a proposed design for minimizing erosion and off-site sedimentation during construction. The erosion and sediment control design will include the temporary placement of appropriate erosion

control BMP's at specific locations along the project. For this scope of work, erosion control BMP's are assumed to be limited to inlet structures, inlet filters and silt fence; however, other erosion control measures may be necessary, dictated by the design and applicable regulations. The plans will reference the DEPARTMENT's Standard Drawings for Roadway Construction to assist the contractor with the construction of these items. The plans will also identify the need to maintain, clean, and relocate these erosion control measures as the project progresses and address the removal of temporary erosion control devices following construction, where applicable. Quantities for erosion and sediment control items will be calculated based on DEPARTMENT typical drawings. Any required erosion control computations will be completed with approved methods and submitted to the COUNTY.

NPDES Permitting – The project will require the acquisition of a National Pollutant Discharge Elimination System (NPDES) permit for construction activities. The NPDES permit is required by the South Carolina Department of Health and Environmental Control (SCDHEC) for all land disturbing activities in South Carolina. The CONSULTANT shall provide all coordination with SCDHEC for the approval of permits.

The CONSULTANT will assist the COUNTY with the development of the NPDES permit application as well as with the submission of any required supporting data. The Stormwater Management Reports for the projects will contain all supporting data developed by the CONSULTANT for the projects. The CONSULTANT will provide additional calculations and make revisions to the construction plans as required by the permit reviewer(s). This scope of services does not include redesign of any elements of the roadway drainage design as a result of comments from the NPDES permit reviewer. Any required revisions would be completed under a separate contract modification.

Task 11

GEOTECHNICAL EXPLORATIONS AND ENGINEERING SERVICES

General – The CONSULTANT will perform final geotechnical exploration for embankments, new slopes and/or retaining walls, shared-use paths, cross-lines culverts, overhead sign foundations, and shoulder widening. The CONSULTANT will gather samples, conduct tests, and analyze necessary soil and foundation data for embankments, new slopes and/or retaining walls, overhead sign foundations and shoulder widening, where applicable. The results of the sampling, testing, analysis, and recommendations concerning the design will be compiled into a final report for submittal to the COUNTY – the preliminary exploration, testing and report development was covered under the original contract; the final report shall be a continuation of the preliminary report. The following design standards will apply:

- 2007 SCDOT Standard Specifications for Highway Construction
- SCDOT Standard Supplemental Specifications and Special Provisions
- 2010 SCDOT Geotechnical Design Manual (GDM), Version 1.1

Field Exploration (Final Subsurface Exploration) – Prior to beginning the final subsurface field exploration, the CONSULTANT will notify the COUNTY seven (7) days in advance so the COUNTY can coordinate with the DEPARTMENT. The CONSULTANT will comply with published DEPARTMENT lane closure restrictions. CONSULTANT has assumed that the COUNTY will obtain permission from property owners for CONSULTANT to perform borings outside of the DEPARTMENT rights-of-way. The CONSULTANT should attempt to locate / perform borings within DEPARTMENT rights-of-way for most instances. The CONSULTANT should provide notification to COUNTY as to which borings may likely need property owner permissions when submitting the Final Boring Plan for approval.

CONSULTANT will request an updated SC811 ticket prior to starting field work for the final exploration.

Final boring locations will be determined by the CONSULTANT. The CONSULTANT will provide copies of the proposed final subsurface exploration plans to the COUNTY prior to initiation of field work for review and acceptance. The testing locations will be coordinated with the preliminary exploration to avoid testing in the same location. See Chapter 4 of the SCDOT GDM for subsurface exploration guidelines. The final subsurface exploration plan is to include, as a minimum, the following:

- Description of the soil or rock stratification anticipated
- Description of the proposed testing types
- Depth of tests
- Location of tests

Embankments, New Slopes and/or Retaining Walls, Shared-Use Paths, Cross-Line Culverts, Overhead Sign Foundations & Shoulder Widening– Subsurface Exploration

- CONSULTANT will have determined location and extent of new retaining walls prior to field work for the final geotechnical exploration.
- CONSULTANT will also have determined the location and invert elevations of new cross line culverts.
- Roadway soil test borings will be performed as specified in the SCDOT Geotechnical Design Manual which references the SCDOT Pavement Design Guidelines for boring frequency. The CONSULTANT has assumed that generally cut and fill sections will be five (5) feet or less in height for the majority of the improvements.
- Final soil test borings will be performed at a frequency of approximately 1,000 feet within the DEPARTMENT's right-of-way or on private property with access permission obtained by the COUNTY. The combined preliminary and final boring spacing should be approximately 500 feet. Retaining walls require a boring every 200 feet. New culvert crossings require borings at each end of the crossing and every 100 feet.

- Four (4) roadway soil test borings (hand auger borings with dynamic cone penetrometers or SPT borings) will be performed up to a depth of 10 feet, auger refusal, or hole collapse (whichever occurs first) inside and/or outside the DEPARTMENT right-of-way.
- Four (4) retaining wall auger probes with temporary piezometers will be performed up to a depth of 40 feet. Auger probes may be conducted within the DEPARTMENT's right-of-way and/or on private property in order to be performed along the alignment of the planned walls. The purpose of the borings is to help better define the water versus potential perched water tables detected during the preliminary exploration. Defining the water and which soils may be submerged during a seismic event will be important to help eliminate liquefaction potential.

Ten (10) Standard Penetration Test borings for sign foundations will be performed up to a depth of forty (40) feet or auger refusal (whichever occurs first) inside and/or outside the DEPARTMENT right-of-way.

Other Field Testing Items

- Traffic control will be performed in accordance with the latest DEPARTMENT guidelines. It is anticipated that four (4) days of lane closures will be necessary.
- At the completion of field work, test locations will be surveyed for latitude and longitude, elevation and station as part of Task 3.

Field Engineering – The CONSULTANT will provide oversight of hand auger borings, drill rig and cone rig operations by a field engineer and/or field geologist. Soil Classification in accordance with USCS (ASTM 2487) will be performed by a field engineer and/or field geologist who will have a minimum of 3-years of experience in supervision of field equipment and field personnel.

Laboratory Testing – The CONSULTANT will be AASHTO certified in the anticipated laboratory testing outlined below and/or any additional testing that may be required. See Chapter 5 of the SCDOT GDM for AASHTO and ASTM designations. The laboratory testing will be performed on selected samples in order to evaluate the types of soils encountered, confirm visual classifications, and estimate engineering properties for use in design. Laboratory testing may include, as estimate, the following:

- 25 Natural Moisture Content Tests
- 25 Grain Size Distributions with wash No. 200 Sieve
- 25 Moisture-Plasticity Relationship Determinations (Atterberg Limits)

Final Roadway Geotechnical Engineering Report – The Final Roadway Geotechnical Engineering Report will be conducted in general accordance with the procedures outlined in the GDM. The report will include a subsurface profile for the final geotechnical subsurface

exploration in accordance with the GDM Chapter 7. The final geotechnical engineering report will be written in general accordance with the GDM Chapter 21. The final report will be signed and sealed by a registered SC Professional Engineer. The report will be submitted with the Preliminary Construction Plans. The Final Report shall be a continuation of the Preliminary Report developed under the original contract.

The CONSULTANT will notify the COUNTY'S designated Project Manager prior to performing any work on site.

This scope of services does not include any work or activities associated with geotechnical investigations for the development of pavement designs. The COUNTY will provide approved pavement design(s) to the CONSULTANT.

Task 12

ROADWAY STRUCTURES DESIGN AND PLANS

General – This task includes design and plan development criteria for proposed retaining walls that will be required due to the proposed improvements for the project. There will be no aesthetic requirements for the retaining walls or culverts. Location and quantities of any temporary shoring required for roadway construction will be included in the roadway construction plans; the shoring design and detailing is the responsibility of the contractor. The following design and construction specifications will be used in the design and preparation of retaining wall and culvert plans:

- The 2007 edition of the DEPARTMENT's *Standard Specifications for Highway Construction*.
- AASHTO's *LRFD Bridge Design Specifications*, 6th edition (2012) and the latest Interim Specifications in place at the time of contract execution.
- AASHTO's *LRFD Bridge Construction Specifications*, 3rd edition (2010) and the latest Interim Specifications in place at the time of contract execution.
- The DEPARTMENT's *Geotechnical Design Manual, v. 1.1, 2010*.
- Supplemental and Technical Supplemental Specifications as already prepared by the DEPARTMENT for design and/or construction.
- DEPARTMENT's Standard Drawings for Road and Bridge Construction.
- DEPARTMENT's *Highway Design Manual (2003)*.
- DEPARTMENT's *Road Design Plan Preparation Guide*.

- AASHTO “Guide Specifications” as may be applicable to the project.

Retaining Wall Design and Plans – Roadway retaining wall(s) likely will be required. The roadway retaining walls are assumed to be cast-in-place, reinforced brick masonry, and/or keystone retaining walls and will be represented in the plans by plan views, envelope drawings, and associated notes and details. Approximately 900 linear feet of retaining wall, at up to 5 separate locations, from 1 feet to 9 feet high, will be required.

Noise wall design is excluded from this scope of services.

Task 13

SUBSURFACE UTILITIES ENGINEERING (SUE)

All Subsurface Utilities Engineering (SUE) services (marking and designating) were conducted under the original contract for the development of Final Right-of-Way Plans. No specific SUE services are anticipated under this service order modification, other than those stated below.

Locating –

No locating services (Level A test holes) are included as a direct service associated with this scope of work. Should locating services be deemed necessary during the design and utility coordination services, these services shall be paid for through the project contingency budget on a per Level A test hole cost.

The services to be conducted by the CONSULTANT, in the performance of locating services, only as directed and by prior approval by the COUNTY, include the following:

- A. In the performance of locating services under this agreement, the CONSULTANT shall,
 1. Provide all equipment, personnel and supplies necessary for the completion of Quality Level A test holes.
 2. Conduct appropriate records and as-built research and investigate site conditions.
 3. Obtain all necessary permits from city, county, state or any other municipal jurisdictions to allow CONSULTANT personnel to work within the existing streets, roads and rights-of-way.
 4. Perform electronic or ground penetrating radar sweep of the proposed conflict and other procedures necessary to adequately “set-up” the test hole.
 5. Excavate test holes to expose the utility to be measured in such a manner that insures the safety of excavation and the integrity of the utility to be measured. In performing such excavations, the CONSULTANT shall comply with all applicable utility damage prevention laws. The CONSULTANT shall schedule and coordinate with the utility companies and their inspectors, as required, and shall be responsible for any damage to the utility during excavation.

6. Provide notification to the COUNTY concerning 1) the horizontal and vertical location of the top and/or bottom of the utility referenced to the project survey datum; 2) the elevation of the existing grade over the utility at a test hole referenced to the project survey datum; 3) the estimated outside diameter of the utility and configuration of non-encased, multi-conduit systems; 4) the utility structure material composition, when reasonably ascertainable; 5) the benchmarks and/or project survey data used to determine elevations; 6) the paving thickness and type, where applicable; 7) the general soil type and site conditions; and 8) such other pertinent information as is reasonable ascertainable from each test hole site.
7. When an attempt to locate a utility line over an area where SUE was performed does not provide valid vertical data, the test hole shall not be reimbursable by the COUNTY. In the following cases, test holes shall be reimbursed by the COUNTY regardless of obtaining valid vertical data:
 - a. Utility lines buried in materials that cannot be removed by vacuum techniques other than duct banks;
The CONSULTANT to provide a separate unit cost for “test holes attempted” and any test holes that do not provide valid vertical data, shall be paid at this rate.
8. Provide permanent restoration of pavement within the limits of the original cut. When test holes are excavated in areas other than roadway pavement, these disturbed areas shall be restored as nearly as possible to the condition that existed prior to the excavation.
9. Draft horizontal location and, if applicable, profile view of the utility on the project plans using CADD standards as outlined above. A station and offset distance and/or northing and easting coordinates (State Plane) with elevations shall be provided with each test hole.
10. Test hole information shall be formatted and presented on CONSULTANT’s certification form and listed in a test hole data summary sheet.
11. Certification form shall be reviewed and sealed by a professional engineer and/or land surveyor licensed in South Carolina and in responsible charge of the project.

B. In the performance of locating services under this agreement, the COUNTY shall,

1. When requested, provide reasonable assistance to the CONSULTANT in obtaining plans showing the project limits, alignment, centerline, rights-of-way limits (existing and proposed), project controls and other data for selected projects.
2. Provide notification to key DEPARTMENT District personnel concerning the upcoming SUE services to be provided by the CONSULTANT.

Task 14

UTILITY COORDINATION ASSISTANCE

The COUNTY will conduct all utility coordination for this project, with assistance by the CONSULTANT, as necessary, specific to attending meetings, updating schedules, and providing

project files. The CONSULTANT will provide electronic copies of available data (ie; CADD files, plans, deeds/plats, etc), as necessary to assist with utility coordination.. The COUNTY'S Utility Coordinator will continue all coordination of the project development with known utility companies during this phase of the project. The CONSULTANT will assume one (1) meeting specific to utility coordination during this phase of work.

Task 15

CONSTRUCTION PHASE SERVICES

The proposed construction phase services shown below are assumed at this time. All necessary construction phase services will be evaluated and negotiated upon completion of the design services tasks and prior to the proposed construction contract. A contract modification will be negotiated for these services.

Pre-Construction/Partnering Conference – The CONSULTANT will attend the Pre-Construction/Partnering Conference and respond to questions by the CONTRACTOR pertinent to the design and proposed construction methodology. Assume attendance at one (1) Pre-Construction/Partnering Conference.

Construction Phase Project Meetings – The CONSULTANT will attend meetings with the COUNTY to discuss construction issues as needed during the construction of this project. Assume attendance at twelve (12) meetings. The CONSULTANT will not be responsible for agendas, minutes, or other materials for this task.

Construction Phase Assistance - The CONSULTANT will assist COUNTY personnel during the construction phase when problems or questions arise relating to the design and proposed construction methodology. Assume 2 hours per month for construction duration of 12 months.

Construction Revisions – The CONSULTANT will make necessary revisions to construction plans that arise during the construction phase of the project. Assume four (4) construction revisions.

As-Built Plans – The CONSULTANT will not be responsible for the development of As-Built Plans for this project.

Services Not Provided

Services not provided by the CONSULTANT include, but are not limited to, the following:

- Landscaping and irrigation plans
- Video Pipe Inspection
- Sight-specific Response Analysis study
- Utility relocation design and plans
- Location of water and sewer utility services for each utility customer in the project
- Right-of-way exhibits
- Right-of-way acquisition, negotiations, or appraisals
- Administering or advertising the bid process
- Fabricating or erecting signs for public meetings
- Alternate designs for bidding
- Construction Engineering and Inspection (CEI)
- Eminent Domain notices
- As-built roadway plans
- Payment of fees required by state and federal review / approval agencies (without reimbursement)
- Pavement Coring or Pavement Designs

Services of the COUNTY

The COUNTY agrees to provide to the CONSULTANT, and at no cost to the CONSULTANT, the following upon request:

- Access to and use of all reports, data and information in possession of the COUNTY which may prove pertinent to the work set forth herein
- Existing Policies and Procedures of the COUNTY with reference to geometrics, standards, specifications and methods pertaining to all phases of the CONSULTANT's work.
- Pavement Design Reports
- Existing roadway plans
- Provide existing signalized intersection coordination timing(s), existing interconnect plan, and location of master, if applicable
- Provide existing utility data provided by utility owners within the project area
- Copies of accident data along the project corridor (ie; crash stack data, accident database info, collision diagrams, etc)
- Eminent Domain advertisement notice
- As-built roadway plans
- Construction Engineering & Inspection (CE&I)
- Right-of-way negotiations & acquisitions
- Right-of-way exhibits
- Construction Documents / Bid document preparation

Schedule

Below is a summary of significant milestones and anticipated submittal timeframes:

Preliminary Construction Plans	3	months from NTP
<i>assume COUNTY review (2 weeks)</i>	3.5	months from NTP
Preliminary Construction Plans (Resubmittal)	4	months from NTP
<i>assume SCDOT reviews **</i>	5	months from NTP
Final Construction Plans	6	months from NTP
<i>assume COUNTY review (2 weeks)</i>	6.5	months from NTP
Final Construction Plans (Resubmittal)	7	months from NTP
<i>assume SCDOT review **</i>	8	months from NTP
Final Construction Plans (Final Submittal)	8.5	months from NTP

The submittal dates include time for COUNTY/DEPARTMENT review as noted.

***Per the Intergovernmental Agreement between the COUNTY and the DEPARTMENT, the DEPARTMENT has 25 business days for their review.*

Clemson / Sparkleberry (2/13/19) - C&D#9

Task	Total	Cox & Dinkins	Davis & Floyd	F&ME	Kimley-Horn	CECS, Inc.
Task 1: Project Management	\$45,860.00	\$28,260.00	\$17,600.00			
Task 2: Environmental / Public Meeting	\$0.00	\$0.00				
Task 3: Field Surveys	\$8,480.00	\$8,480.00				
Task 4: Roadway Design	\$61,200.00	\$13,800.00	\$47,400.00			
Task 5: Pavement Marking & Signing Plans	\$49,200.00	\$18,000.00	\$31,200.00			
Task 6: Traffic Signal Design	\$75,675.00	\$14,400.00			\$61,275.00	
Task 7: Lighting Plan	\$24,400.00	\$2,400.00	\$22,000.00			
Task 8: Work Zone Traffic Control Design	\$117,800.00	\$32,400.00	\$85,400.00			
Task 9: Stormwater / Hydraulic Design	\$24,400.00	\$24,400.00				
Task 10: Sediment / Eros Control / NPDES	\$25,600.00	\$25,600.00				
Task 11: Geotechnical Invest & Engineering	\$50,178.00	\$3,600.00		\$46,578.00		
Task 12: Roadway Structures Design	\$38,870.00	\$38,870.00				
Task 13: SUE	\$0.00	\$0.00				
Task 14: Utility Coord Assistance	\$4,200.00	\$4,200.00				
Task 15: Construction Phase Assistance	\$0.00	\$0.00				
Total	\$525,863.00	\$214,410.00	\$203,600.00	\$46,578.00	\$61,275.00	\$0.00
Total %	100.0%	40.8%	38.7%	8.9%	11.7%	0.0%

DBE Certified						x
SLBE Certified		x				x

DBE Utilization	0.0%
SLBE Utilization	40.8%

Lump Sum	\$488,498.00
Approved Direct Expenses	\$37,365.00
Cost Plus Fixed Fee (R/W Staking)	\$0.00
Cost Plus Fixed Fee (SUE Surveys)	\$0.00
Total	\$525,863.00

		Directs		
F&ME		geotech	\$31,365.00	final geotech field & lab
Kimley-Horn		traffic signals	\$6,000.00	(6) turning movement counts

Total Directs \$37,365.00

Service Order
For
On Call Engineering Services Agreement

SERVICE ORDER NO. CECS#8

Date: February 13, 2019

This Service Order No. CECS#8 is issued by Richland County, South Carolina (the “County”), to CECS, Inc. (the “Consultant”) pursuant to that Agreement dated February 11, 2015 between the County and the Consultant called “On Call Engineering Services Agreement Related to the Richland County, South Carolina Sales Tax Public Transportation Improvement Plan” (the “Agreement”).

This Service Order, together with the Agreement, form a Service Agreement. A Service Agreement represents the entire and integrated agreement between the parties and supersedes prior negotiations, representations, or agreements, either written or oral. A Service Agreement may be amended or modified only by a Change Order or Change Directive as provided for in the Agreement.

I. Scope of Services.

A. Unless otherwise provided in an exhibit to this Service Order, this Service Order and the Service Agreement are based on the information set forth below:

See Exhibit A – Scope of Services

B. Unless otherwise provided in an exhibit to this Service Order, the Consultant’s Services to be provided pursuant to this Service Order are:

See Exhibit A – Scope of Services

C. Unless otherwise provided in an exhibit to this Service Order, the County’s anticipated dates for commencement of the Services and Completion of the Services are set forth below:

1. Commencement Date: March 8, 2019
2. Completion Date: *See Exhibit A – Scope of Services - Schedule*

D. Key personnel assigned by Consultant to this Service Scope of Work:

1. Paul Raad, P.E. (Principal in Charge)
2. Brian Nickerson, P.E. (Project Manager)

II. Insurance

The Consultant shall maintain insurance as set forth in the Agreement. If the Consultant is required to maintain insurance exceeding the requirements set forth in the Agreement, those additional requirements are as follows:

N/A

III. Owner’s Responsibilities.

In addition to those responsibilities the County may have as stated in the Agreement, the County in connection with this Service Order only shall:

N/A

IV. Consultant’s Compensation.

A. The Consultant shall be compensated for Services provided under this Service Order as follows:

<i>Lump Sum</i>	\$	238,696.43
<i>Approved Direct Expenses</i>	\$	<u>0.00</u>
		238,696.43
 <i>Contingency – Not to Exceed¹</i>	 \$	 23,869.64

¹ Requires approval from Richland County to authorize contingency

B. Additional Services. Unless otherwise provided in an exhibit to this Service Order, any Additional Services by the Consultant shall be paid as Additional Services as provided in the Agreement.

V. Additional Exhibits.

The following exhibits and/or attachments are incorporated herein by reference thereto:

Exhibit A – Scope of Services

VI. Execution of Service Agreement

The Execution of this Service Order by the County below constitutes a Service Order to the Consultant. The execution of this Service Order by the Consultant creates the Service Agreement.

NOW, THEREFORE, in consideration of the foregoing, the sufficiency of which is hereby acknowledged by the parties, this Service Agreement is entered into Under Seal as of the Effective Date of _____, 2019.

WITNESS:

RICHLAND COUNTY, SOUTH CAROLINA

By: _____ (L.S.)

Its: _____

Date: _____

CONSULTANT WITNESS:

CECS, Inc.

Shelagh Cobb

By: *Curtis Paul* _____ (L.S.)

Its: *President* _____

Date: *2/15/19* _____

EXHIBIT A: SCOPE OF SERVICES

ATTACHMENT "A"
SCOPE OF SERVICES AND SCHEDULE
BROAD RIVER RD (US RTE 76) WIDENING
SERVICE ORDER MODIFICATION #1

CECS, Inc. (CONSULTANT) has been authorized by Richland County (COUNTY) to provide engineering services for the widening of Broad River Road (US 76) in Richland County, South Carolina. Broad River Rd (US 76) is considered an Urban Minor Arterial by the South Carolina Department of Transportation (DEPARTMENT). The DEPARTMENT holds all public rights-of-way adjacent to the project corridor and assumes all maintenance responsibilities for those said rights-of-way. Some of the project area is also within the limits of the Town of Irmo (CITY). The project will consist of widening the existing roadway to five lanes between Royal Tower Drive (S-1862) and Dutch Fork Road (US 76) to include bicycle and pedestrian accommodations.

The scope of this service order modification (SOM#1) amends the scope for Service Order No. CECS#4 (September 25, 2017) and includes Project Management, Field Surveys, Roadway Design, Traffic Signal Design, Stormwater Management/Hydraulic Design, and Sediment and Erosion Control/NPDES Permitting. All services and requirements of the scope (CECS#4) remain in affect except as modified herein.

The work included in this SOM#1 is specific to necessary design and plan updates as determined from the previous preliminary plan design verifications, recommendations resulting from property and rights-of-way impact reviews and required revisions specific to other agency coordination(s).

Task 1

PROJECT MANAGEMENT

The anticipated design schedule will be increased from 22 months to 26 months. This will result in the following tasks for the CONSULTANT:

- ◆ Four (4) additional project meetings and associated tasks.
- ◆ Four (4) additional monthly invoices, status reports, and schedule updates.
- ◆ Four (4) additional months of coordination with its SUB-CONSULTANTS during the execution of their work.

Task 3

FIELD SURVEYS

The following will be added:

Existing Pavement Surveys – The existing pavement will be field surveyed in accordance with the SCDOT Preconstruction Survey Manual and provide data sufficient for the design, permitting and construction of the project. Cross sections will be surveyed at a minimum of every 50 feet, including edge of pavement, crown, and other breaks in cross slope or edge of pavement. Existing pavement surveys will extend along US 76 from 200 feet east of Royal Tower Drive (S-1862) to 200 feet west of Walmart Road and along all intersecting roadways for 300 feet, with the exception of the following

- Woodrow Street (S-27) – 800 feet
- Koon Road (S-58) – 500 feet
- US 176 – from US 76 to Walmart Road
- Salem Church Road (S-56) – 1,100 feet

It is assumed that all previous ground control is recoverable for use with these supplemental surveys.

It is assumed that the surveys conducted under this contract modification will be incorporated into the existing project surface DTM. The updated and incorporated DTM file (CADD file, .tin file, etc) will be provided to the COUNTY by the CONSULTANT.

The CONSULTANT shall be entitled to rely, without liability, on the accuracy and completeness of any and all information provided by the COUNTY and the COUNTY'S consultants for all work not stated specifically in the scope above within this task.

Task 4

ROADWAY DESIGN

The following will be added:

Rights-of-Way Plans – The Rights-of-Way Plans (beginning with Preliminary Rights-of-Way Plans) will be revised / updated by the CONSULTANT and submitted for COUNTY review and approval. The following design revisions are anticipated:

Design Task 1: Develop new US 76 profile based on approved pavement design for pavement overlay.

Design Task 2: US 76 alignment shift and necessary design, plan and cross-section updates to eliminate parking impacts to tracts 3, 4, 5 & 6 (parcel numbers as denoted on preliminary plans). It is proposed to shift the alignment / widening to the north side of the roadway to eliminate parking impacts.

Design Task 3: US 76 alignment shift and necessary design, plan and cross-section updates to eliminate all property and / or rights-of-way impacts to the John Jacob Calhoun Koon Farmstead property, tract 46 (parcel number as denoted on preliminary plans). It is proposed to shift the alignment / widening to the north side of the roadway to eliminate associated impacts.

Design Task 4: Add a right turn lane to US 76 westbound near station 36+00.

Design Task 5: Add a right turn lane to Caedmons Creek Drive.

Design Task 6: Update design, plan and cross-sections to reflect approved Koon Road lane geometry per traffic study, including associated dedicated right turn lane from Broad River Road.

Design Task 7: Provide side road designs, as necessary, including design criteria, typical sections, baselines, profiles, cross sections, and truck turning templates.

Design Task 8: Realign Quattlebaum Road to tie into the Woodrow Street intersection.

Design Task 9: Realign Elliot Richardson Road or Ministry Drive as necessary.

Design Task 10: Revise turn lane storage lengths based on the Final Traffic Report to be provided by the COUNTY.

Design Task 11: Revise superelevation for US 76 curve between approximate stations 65 and 74.

Design Task 12: Add raised concrete median island adjacent to US 76 left turn lane to Koon Road.

Design Task 13: Revise US 76 / US 176 intersection based on the Final Traffic Report to be provided by the COUNTY.

Design Task 14: Clarify property access and show existing pavement removal along Salem Church Road.

The Design Tasks noted above involve related revisions including reference data sheets, plan sheets, profile sheets, and cross sections.

Task 6**TRAFFIC SIGNAL DESIGN**

Traffic Signal plans for the following intersections will be added:

- Broad River Road (US 76) at Royal Tower Drive (S-1862) - new signal
- Broad River Road (US 76) at Farming Creek Road (S-957) – new signal

Task 8**STORMWATER MANAGEMENT/HYDRAULIC DESIGN**

Roadway Design Tasks noted above will be incorporated into the scope for this task.

Task 9**SEDIMENT AND EROSION CONTROL/NPDES PERMITTING**

Roadway Design Tasks noted above will be incorporated into the scope for this task.

Schedule

The anticipated schedule will be revised as follows:

Field Surveys	2	months from NTP
Preliminary Right-of-Way Plans	6	months from NTP
<i>assume COUNTY review (1 month)</i>	7	months from NTP
<i>assume SC DOT review (1 month)</i>	8	months from NTP

Subsequent project delivery will follow the schedule as shown in the original contract.

The submittal dates include time for COUNTY/DEPARTMENT review as noted. Per the Intergovernmental Agreement between the COUNTY and the DEPARTMENT, the DEPARTMENT has 25 business days for their review.

Broad River Rd (US 176) Widening - (2/13/19) - CECS#8

Task	Total	CECS, Inc	Cox & Dinkins	New South	S&ME	Parrish & Partners, LLC	Chao & Assoc.	PJNA	CECS, Inc. (DBE)
Task 1: Project Management	\$22,341.76	\$22,341.76							
Task 2: Environmental / Public Meeting	\$0.00	\$0.00							
Task 3: Field Surveys	\$49,334.28	\$0.00	\$49,334.28						
Task 4: Roadway Design	\$142,611.83	\$142,611.83							
Task 5: Pavement Marking & Signing Plans	\$0.00	\$0.00							
Task 6: Traffic Signal Design	\$24,408.56	\$24,408.56							
Task 7: Transportation Management Plan	\$0.00	\$0.00							
Task 8: Stormwater / Hydraulic Design	\$0.00	\$0.00							
Task 9: Sediment / Eros Control / NPDES	\$0.00	\$0.00							
Task 10: Geotechnical Invest & Engineering	\$0.00	\$0.00							
Task 11: Roadway Structures	\$0.00	\$0.00							
Task 12: SUE	\$0.00	\$0.00							
Task 13: Utility Coord Assistance	\$0.00	\$0.00							
Task 14: Construction Phase Assistance	CONSTRUCTION PHASE SERVICES TO BE CONDUCTED PER FUTURE CONTRACT MODIFICATION								
Total	\$238,696.43	\$189,362.15	\$49,334.28	\$0.00	\$0.00	\$0.00	\$0.00	\$0.00	\$0.00
Total %	100.0%	79.3%	20.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%

DBE Certified				x			x	x	x
SLBE Certified	x		x	x		x	x	x	x

DBE Utilization	0.0%
SLBE Utilization	100.0%

Lump Sum	\$238,696.43
Approved Direct Expenses	\$0.00
Total	\$238,696.43

Directs

Total Directs \$0.00



TRANSPORTATION PROGRAM

February 5, 2019

Dr. John Thompson
Director of Transportation
Richland County Government
P.O. Box 192
Columbia, South Carolina 29202

Re: Broad River Neighborhood Improvements
PDT-325-IFB-2019

Dear Dr. Thompson:

A bid opening was held at 2:00 PM on Wednesday, January 23, 2019 at the Richland County Office of Procurement at 2020 Hampton Street for the Broad River Neighborhood Improvements Project. The Richland Program Development Team has reviewed the three (3) submitted bids for Broad River Neighborhood Improvements which were submitted via Bid Express and found no discrepancies. The bids received were as follows.

BROAD RIVER NEIGHBORHOOD IMPROVEMENTS - BID RESULTS SUMMARY	
BIDDER	SUBMITTED BID
Cherokee, Inc.	\$ 858,063.50
AOS Specialty Contractors, Inc.	\$1,012,140.00
Palmetto Corporation of Conway, Inc.	\$ 1,168,934.65

Further review shows that the Cherokee, Inc. is duly licensed in South Carolina to perform this work. A copy of their license is attached.

A Mandatory Pre-Bid Conference was held at 10:00 AM on December 19, 2018 during which attendees gained information and bidding directives for the project. The Sign-In Sheet for the Pre-Bid Meeting is attached indicating interested firms that were in attendance.

Attached is a final bid tab sheet for your reference which indicates Cherokee's bid to be 17.9% below the Engineer's Estimate of \$1,045,660.36 for the project. A review of the low bid also shows a 19% Disadvantaged Business Enterprise (DBE) utilization commitment which exceeds the 17% goal identified for this project. Cherokee, Inc. has also submitted the required DBE Utilization Form indicating the DBE firms will be participating in the work.

In accordance with the guidelines for this project, SCDOT approval and concurrence is required for the project to move forward. After forwarding all required information to the agency to include bid information, Engineer's estimate comparisons, DBE Committal information, and other required forms, we have received their formal concurrence as attached.

Therefore; Richland PDT recommends that a contract be awarded to the lowest responsive and responsible bidder, Cherokee, Incorporated. It is further recommended that the approval of the award also include a 10% contingency of \$ 85,806.35. We will schedule the pre-construction conference once we have been notified by you that Council has approved the contract.

Sincerely,



Dale Collier
Procurement Manager
Richland PDT, A Joint Venture

Cc: Taylor Neely, Richland PDT
Jennifer Wladischkin, Richland County

ATTACHMENTS:

Certified Bid Tab
Bid Form – Cherokee, Inc.
Bid Comparison to Engineering Estimate
Pre-Bid Sign In Sheets
Cherokee, Inc. License Confirmation
Cherokee, Inc. DBE Utilization Documentation
SCDOT Award Concurrence Form

LPA Construction Award Concurrence Request Form

Prior to the award of a construction contract, the Local Public Agency (LPA) shall submit to the SCDOT Local Public Agency Administrator (LPAA), this request for review and concurrence of award along with the appropriate documentation listed below. Upon review and concurrence, the LPA shall provide a formal award notice to the successful bidder.

Local Public Agency: Richland County
 Contact Person: Dr. John Thompson Title: Director of Transportation
 Address: 2000 Hampton Dt., Columbia, SC 29204 Phone: 803.766.5003
email -thompson.john@richlandcountysc.gov

Contract Area / Number: _____ County: Richland

Project Name: Broad River Neighborhood Improvement Project
 File Number: P029404 Master PIN Number: _____
 Date of Agreement: October 18, 2017 Construction Completion Date: _____

To: **Director of Construction**

The County/City/Town of Richland is requesting your concurrence in award for the construction of the above referenced project to Cherokee, Inc. The project description is as follows:

The project consist of 0.674 miles of new sidewalk and associated handicap ramps along Gibson Street, Hart Street, Pearl Street and McRae Street within the Broad River Neighborhood. In addition to sidewalks, the project includes landscaping, irrigation, stormwater drainage sytem, curb and gutter installation, striping, traffic control, grading, concrete driveways, sodding and erosion control.

This concurrence request shall include the following documentation:

Construction:

- SCBO Advertisement
- Bid Proposal (include electronic and hard copy)
- Plans
- Construction Estimates (include electronic and hard copy)
- List of Bidders with Bid Amounts
- Bid Tabulations (include electronic and hard copy)
- Bid Bonds
- Committal Sheets for DBE participation (if required)
- Signed Quotes from DBE(s) listed on Committal Sheet (if required)

The South Carolina Department of Transportation has reviewed the documentation submitted and concurs with the selection of Cherokee, Inc. as the successful proposer / bidder. The County/City/Town of Richland may award the contract to Cherokee, Inc. based on the information submitted by you on Jan 30, 2019.

Construction:

- Concur Concurrence Denied

WHS


 Director of Construction

2-5-2019

 Date



TRANSPORTATION PROGRAM

February 5, 2019

Dr. John Thompson
Director of Transportation
Richland County Government
P.O. Box 192
Columbia, South Carolina 29202

Re: Dirt Road Package I
PDT-747-IFB-2019

Dear Dr. Thompson:

A bid opening was held at 2:00 PM on Wednesday, January 30, 2019 at the Richland County Office of Procurement at 2020 Hampton Street for the Dirt Road Package I Project. The Richland Program Development Team has reviewed the five (5) submitted bids for Dirt Road Package I which were submitted via Bid Express and found no discrepancies. The bids received were as follows.

DIRT ROAD PACKAGE I - BID RESULTS SUMMARY	
BIDDER	SUBMITTED BID
R & T Grading, Inc.	\$ 952,811.84
McClam & Associates	\$1,128,763.09
Armstrong Construction	\$1,197,623.75
AOS Specialty Contractors	\$1,289,112.68
Cherokee, Inc.	\$1,425,773.00

Further review shows that the R & T Grading, Inc. is duly licensed in South Carolina to perform this work. A copy of their license is attached.

A Mandatory Pre-Bid Conference was held at 10:00 AM on January 9, 2019 during which attendees gained information and bidding directives for the project. The Sign-In Sheet for the Pre-Bid Meeting is attached indicating interested firms that were in attendance.

Attached is a final bid tab sheet for your reference which indicates R & T Grading, Inc.'s bid to be 29.2% below the Engineer's Estimate of \$1,345,127.23 for the project. A review of the low bid also shows a commitment of **5.93%** utilization of Small Local Business Enterprise (SLBE) companies which approximately equals the 5.95% goal identified for this project. R & T Grading, Inc. has also submitted the required SLBE Utilization Form indicating the SLBE firms will be participating in the work.

R & T Grading's SLBE commitment of 5.93% is just shy of the 5.95% goal which equates to a deficit of \$208.30 on the approximate \$1M project. If R & T Grading is not awarded the project, Richland County would move to the next low bidder, McClam & Associates, and incur additional costs totaling \$175,951.25. In response to our inquiry, R & T Grading has provided their attached written commitment to increase their SLBE percentage to meet the indicated goal at no additional cost to the County.

Therefore; Richland PDT recommends that a contract be awarded to the lowest responsive and responsible bidder, R & T Grading, Inc. with the stipulation that this firm provide a plan to meet the 5.95% SLBE Utilization Commitment in its entirety without altering its total bid price. It is further recommended that the approval of the award also include a 10% contingency of \$ 95,281.18. We will schedule the pre-construction conference once we have been notified by you that Council has approved the contract.

Sincerely,



Dale Collier
Procurement Manager
Richland PDT, A Joint Venture

Cc: Taylor Neely, Richland PDT
Jennifer Wladischkin, Richland County

ATTACHMENTS:

Certified Bid Tab
Bid Form – R & T Grading, Inc.
Bid Comparison to Engineering Estimate
Pre-Bid Sign In Sheets
R & T Grading, Inc. License Confirmation
R & T Grading, Inc. DBE Utilization Documentation
R & T Grading, Inc. DBE Utilization Commitment email

A Mandatory Pre-Bid Conference was held at 10:00 AM on January 30, 2019 during which attendees gained information and bidding directives for the project. The Sign-In Sheet for the Pre-Bid Meeting is attached indicating interested firms that were in attendance.

Attached is a final bid tab sheet for your reference which indicates the low bid to be 22.12% below the Engineer's Estimate of \$4,699,996.21 for the project. A review of the low bid also shows a commitment of **17.5%** utilization of Small Local Business Enterprise (SLBE) companies which exceeds the 17.3% goal for this project.

Richland PDT recommends that a contract be awarded to the lowest responsive and responsible bidder, McClam & Associates. It is further recommended that the approval of the award also include a 10% contingency of \$366,032.68. We will schedule the pre-construction conference once we have been notified by you that Council has approved the contract.

Sincerely,

RICHLAND PDT, A JOINT VENTURE



Dale Collier
Procurement Manager
Richland PDT, A Joint Venture

Cc: Taylor Neely, Richland PDT
Jennifer Wladischkin, Richland County

ATTACHMENTS:

Certified Bid Tab

Bid Form – McClam & Associates

Bid Comparison to Engineering Estimate

Pre-Bid Sign In Sheets

McClam & Associates / Taylor Brothers License Confirmation

McClam & Associates SLBE Participation Sheet



TRANSPORTATION PROGRAM

February 5, 2019

Dr. John Thompson
Director of Transportation
Richland County Government
P.O. Box 192
Columbia, South Carolina 29202

Re: Broad River Neighborhood Improvements
PDT-325-IFB-2019

Dear Dr. Thompson:

A bid opening was held at 2:00 PM on Wednesday, January 23, 2019 at the Richland County Office of Procurement at 2020 Hampton Street for the Broad River Neighborhood Improvements Project. The Richland Program Development Team has reviewed the three (3) submitted bids for Broad River Neighborhood Improvements which were submitted via Bid Express and found no discrepancies. The bids received were as follows.

BROAD RIVER NEIGHBORHOOD IMPROVEMENTS - BID RESULTS SUMMARY	
BIDDER	SUBMITTED BID
Cherokee, Inc.	\$ 858,063.50
AOS Specialty Contractors, Inc.	\$1,012,140.00
Palmetto Corporation of Conway, Inc.	\$ 1,168,934.65

Further review shows that the Cherokee, Inc. is duly licensed in South Carolina to perform this work. A copy of their license is attached.

A Mandatory Pre-Bid Conference was held at 10:00 AM on December 19, 2018 during which attendees gained information and bidding directives for the project. The Sign-In Sheet for the Pre-Bid Meeting is attached indicating interested firms that were in attendance.

Attached is a final bid tab sheet for your reference which indicates Cherokee's bid to be 17.9% below the Engineer's Estimate of \$1,045,660.36 for the project. A review of the low bid also shows a 19% Disadvantaged Business Enterprise (DBE) utilization commitment which exceeds the 17% goal identified for this project. Cherokee, Inc. has also submitted the required DBE Utilization Form indicating the DBE firms will be participating in the work.

In accordance with the guidelines for this project, SCDOT approval and concurrence is required for the project to move forward. After forwarding all required information to the agency to include bid information, Engineer's estimate comparisons, DBE Committal information, and other required forms, we have received their formal concurrence as attached.

Therefore; Richland PDT recommends that a contract be awarded to the lowest responsive and responsible bidder, Cherokee, Incorporated. It is further recommended that the approval of the award also include a 10% contingency of \$ 85,806.35. We will schedule the pre-construction conference once we have been notified by you that Council has approved the contract.

Sincerely,

A handwritten signature in blue ink that reads "Dale Collier". The signature is fluid and cursive.

Dale Collier
Procurement Manager
Richland PDT, A Joint Venture

Cc: Taylor Neely, Richland PDT
Jennifer Wladischkin, Richland County

ATTACHMENTS:

Certified Bid Tab
Bid Form – Cherokee, Inc.
Bid Comparison to Engineering Estimate
Pre-Bid Sign In Sheets
Cherokee, Inc. License Confirmation
Cherokee, Inc. DBE Utilization Documentation
SCDOT Award Concurrence Form

LPA Construction Award Concurrence Request Form

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Local Public Agency: Richland County
 Contact Person: Dr. John Thompson Title: Director of Transportation
 Address: 2000 Hampton Dt., Columbia, SC 29204 Phone: 803.766.5003
email -thompson.john@richlandcountysc.gov

Contract Area / Number: _____ County: Richland

Project Name: Broad River Neighborhood Improvement Project
 File Number: P029404 Master PIN Number: _____
 Date of Agreement: October 18, 2017 Construction Completion Date: _____

To: **Director of Construction**

The County/City/Town of Richland is requesting your concurrence in award for the construction of the above referenced project to Cherokee, Inc. The project description is as follows:

The project consist of 0.674 miles of new sidewalk and associated handicap ramps along Gibson Street, Hart Street, Pearl Street and McRae Street within the Broad River Neighborhood. In addition to sidewalks, the project includes landscaping, irrigation, stormwater drainage sytem, curb and gutter installation, striping, traffic control, grading, concrete driveways, sodding and erosion control.

This concurrence request shall include the following documentation:

Construction:

- SCBO Advertisement
- Bid Proposal (include electronic and hard copy)
- Plans
- Construction Estimates (include electronic and hard copy)
- List of Bidders with Bid Amounts
- Bid Tabulations (include electronic and hard copy)
- Bid Bonds
- Committal Sheets for DBE participation (if required)
- Signed Quotes from DBE(s) listed on Committal Sheet (if required)

The South Carolina Department of Transportation has reviewed the documentation submitted and concurs with the selection of Cherokee, Inc. as the successful proposer / bidder. The County/City/Town of Richland may award the contract to Cherokee, Inc. based on the information submitted by you on Jan 30, 2019.

Construction:

- Concur Concurrence Denied

WHS


 Director of Construction

2-5-2019

 Date

EXECUTIVE SUMMARY

Date: January 22, 2019

To: Dr. John Thompson
Director of Transportation

From: David Beaty, PE
Program Manager

RE: Calhoun Road Diet – Public Meeting Summaries with Recommendation

Introduction

The Calhoun Road Diet for Bike Accommodations Project is one of four Road Diet for Bike Accommodations Projects included in the 2012 Referendum, with a budgeted amount of \$88,292. The Richland Penny PDT coordinated with the City of Columbia to design road diets that would create safe bike lanes; but to do so will require removal of some parking on the north side of the road. The purpose of this document is to summarize the public input process and provide a recommendation to implement this project in accordance with public responses to advance the project.

In order to solicit public input, the Richland County Transportation Program, along with the City of Columbia, completed conceptual studies with a concept report and conducted two public meetings; one for the Calhoun and Hampton Road Diet on June 28, 2018, and a follow up meeting for just Calhoun on November 7, 2018.

The two public meetings followed an informal, open-house format, and included project displays highlighting aerial overview maps, typical road diet sections, and overall benefits of road diets. PDT, City and County staff attended to answer questions and gather comments from the public. Staff provided comment cards for written comments and questions for the public to answer to gauge the amount of support for the project.

June 28, 2018 Public Meeting

The Richland County Transportation Program held a public meeting for the Calhoun and Hampton Road Diet plans on Thursday, June 28, 2018 from 6:00 to 8:00 at the Holy Trinity Greek Orthodox Cathedral located at 1931 Sumter St. This plan includes a road diet, where the four-lane road will be restriped to create a two-lane road with a center turn lane; and sharrows, where vehicle and bike traffic share the road when there is not enough room for a bike lane.

Staff received 49 comments, with more than 60% in favor of the project. Four comments received were opposed to the plan because implementation would result in lost parking along Calhoun near St. Timothy's Episcopal Church (Calhoun and Lincoln intersection). The PDT revised the plans to keep parking on this block in response to these comments. The PDT also received three comments from Transitions Homeless Center that sixteen overnight parking permits granted by the City would be lost. The City confirmed that the spots would be moved to the other side of Calhoun St. or side streets and no spots would be lost.

After the meeting, City and PDT Staff presented their findings to the City and County Councils. The City Council requested, and County Council concurred, the City and PDT to conduct another meeting for just Calhoun Street, and to postpone the Hampton Road Diet until after the construction of Calhoun.

November 7, 2018 Public Meeting

The Richland County Transportation Program held a public meeting for the Calhoun Road Diet on Thursday, November 7, 2018 from 6:00 to 8:00 p.m. at the Holy Trinity Greek Orthodox Cathedral, located at 1931 Sumter Street. The proposed improvements included road diets and sharrows. The road diet concept would be implemented on Calhoun from Park to Assembly and from Sumter to Pickens. Also included is the removal of the north side parking along the blocks within the road diet and Assembly to Sumter, to create wider vehicle travel lanes, and to provide additional width for bike lanes. Sharrows will connect the road diets along Calhoun from Wayne to Park and from Pickens to Harden and do not require removing existing parking.

Staff received 42 comments during the comment period. 31 comments were in favor, 8 were against, and 3 were for bike lanes but did not favor the current plan.

Interestingly, three comments were on behalf of the Episcopal Church from citizens who were previously against the plan, but now are in favor of it, since the parking across from the church will now be preserved. Six comments shared concerns for the loss of parking on Calhoun.

Recommendation

The Richland PDT and City of Columbia request that the Calhoun Street Road Diet be advanced through design and to construction as presented at the November 7, Public Meeting to include resurfacing of the roadway between Park Street and Pickens Street at an estimated cost of \$1.5 Million.

Appendix A: November 7, 2018 Public Meeting Information
Appendix B: Public Comments
Appendix C: Concept Report

Calhoun Street Road Diet



Will remove parking on north side of the street in green section (21 spaces)

No parking removed in red line section.

No parking removed in red line section.

Legend

Bike Lane/
Road Diet —

Sharrow —



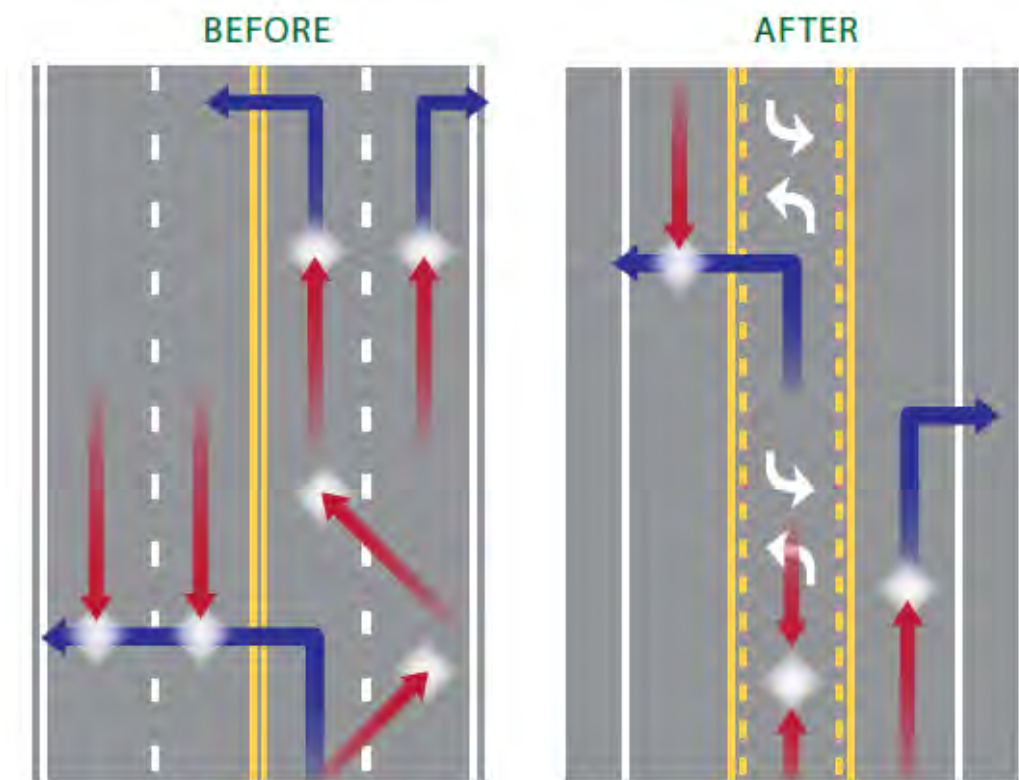
Road Diet Benefits

I. Designated left turn lane prevents through traffic from being restricted.

II. Center turn lane for efficient turning movements.

III. Four 9' lanes causes shy driving and cars unable to drive side by side. Three 11' lanes give drivers more room resulting in more comfortable driving conditions.

IV. Wider lanes will reduce sideswipe accidents and create a safer road.



"Road Diet Guide - Car Free America". Car Free America. Retrieved 2018-10-31.

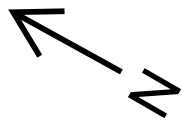
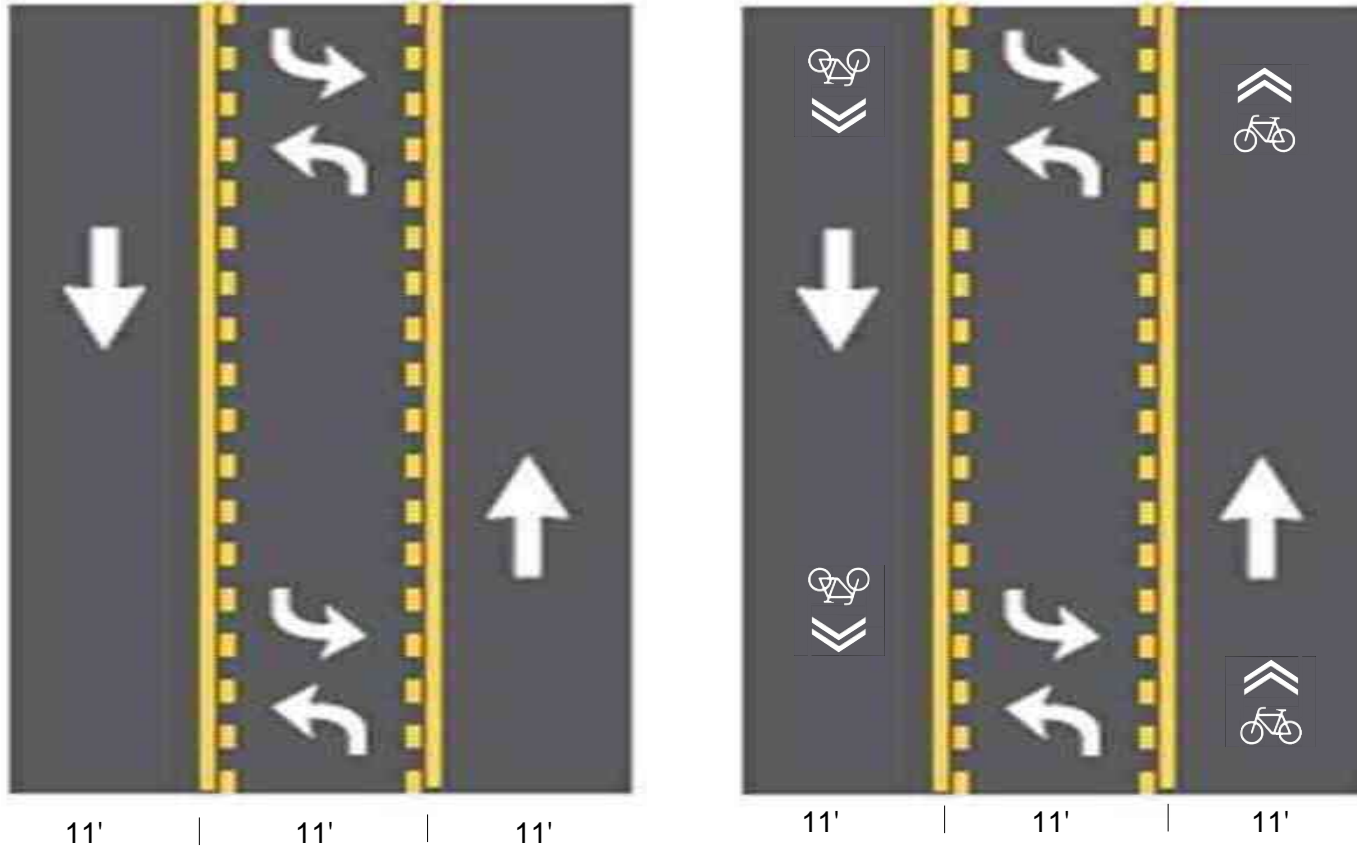
Calhoun Street Road Diet

33' Typical Section
Barnwell to Harden

No lane changes or parking removal will occur in the 33' sections of Calhoun. Instead it will be marked as a sharrow.

Existing

Proposed

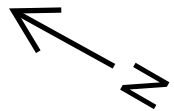


*Not to scale

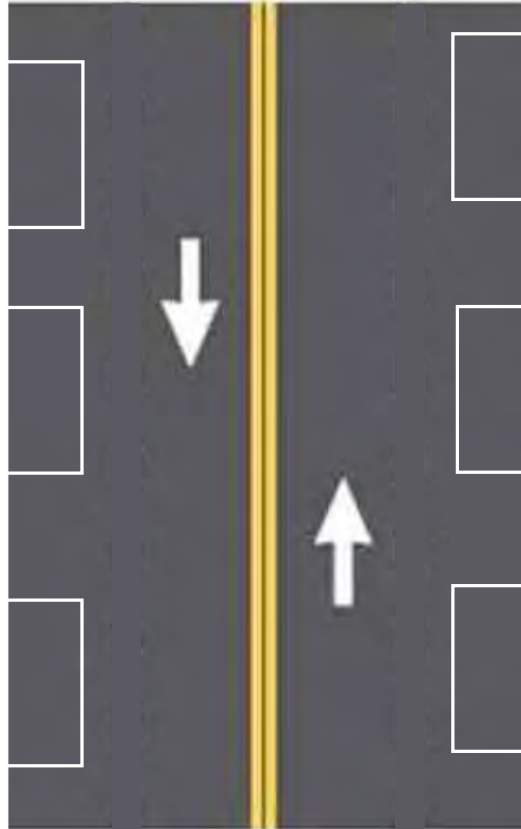
Calhoun Street Road Diet

33' Typical Section
(Wayne to Park and Pickens to Barnwell)

No lane changes or parking removal will occur in the 33' sections of Calhoun. Instead it will be marked as a sharrow.

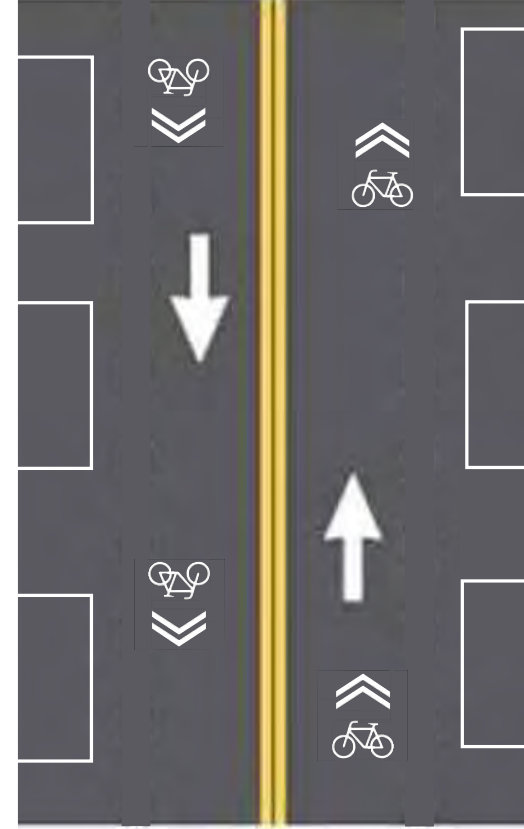


Existing



6' | 10.5' | 10.5' | 6'

Proposed



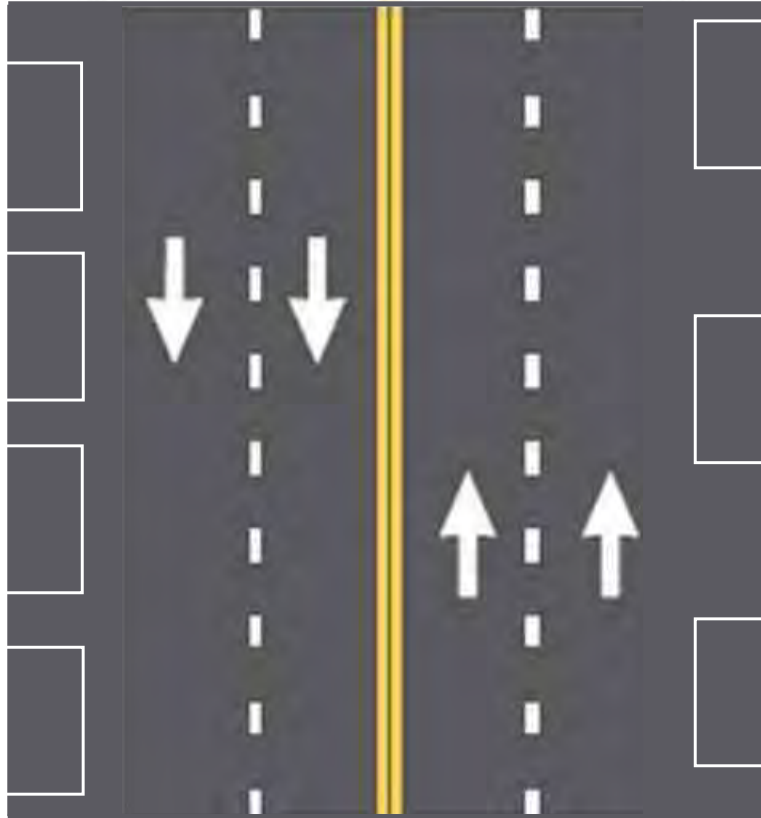
6' | 10.5' | 10.5' | 6'

*Not to scale

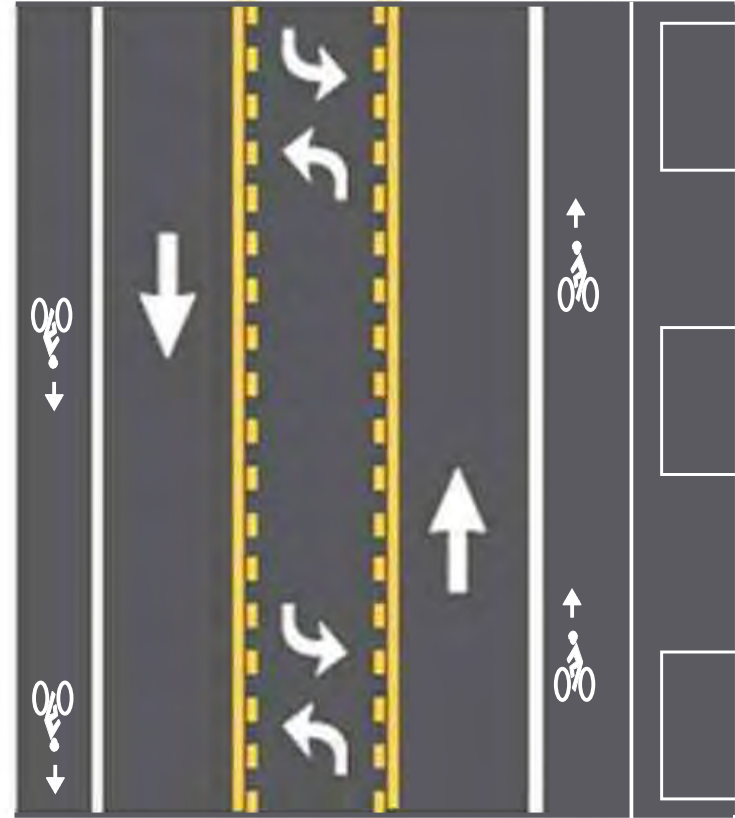
Calhoun Street Road Diet

48' Typical Section
(Park to Assembly and Sumter to Pickens)

Existing



Proposed



*Not to scale

6' | 9' | 9' | 9' | 9' | 6'

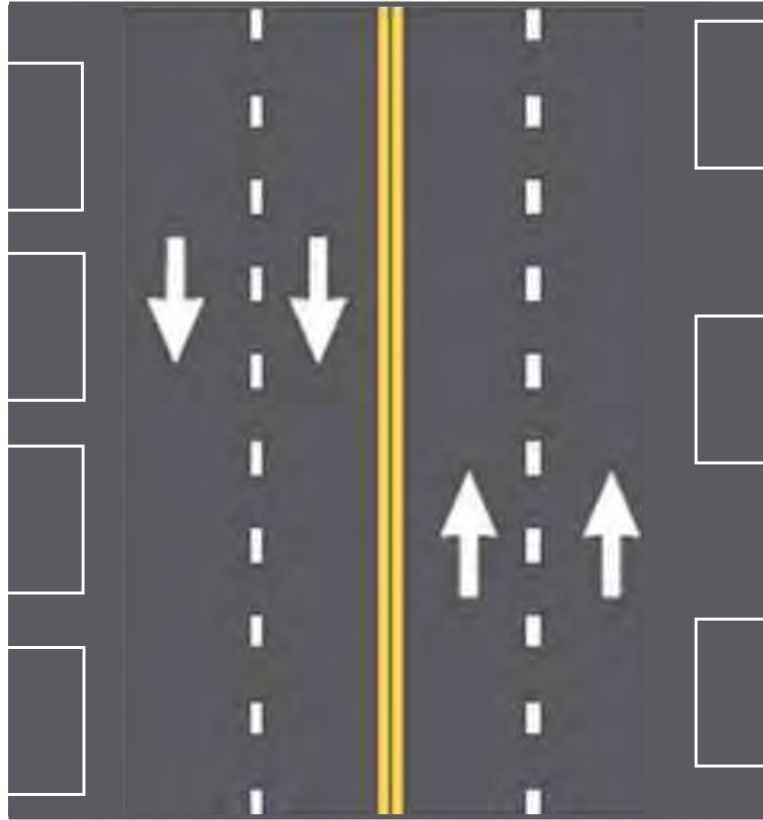
*Removes 21 metered parking spots

4' | 11' | 11' | 11' | 5' | 6' |

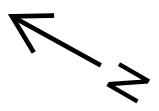
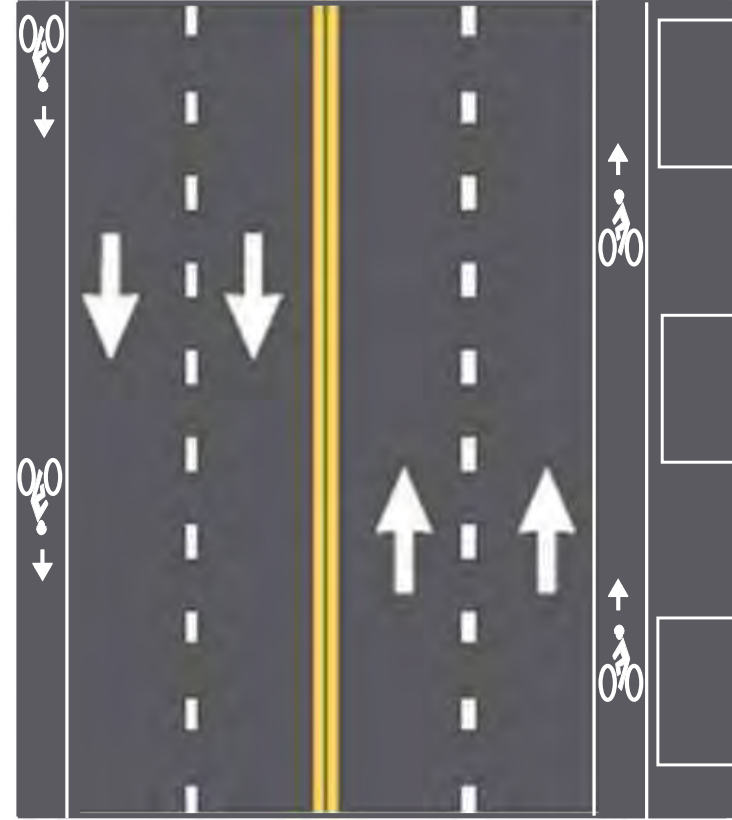
Calhoun Street Road Diet

62' Typical Section
Assembly to Sumter

Existing



Proposed



*Not to scale 6' | 13' | 12' | 12' | 13' | 6'

4' | 12' | 11.5' | 11.5' | 12' | 5' | 6'

*Removes 21 metered parking spots



CALHOUN STREET (WAYNE ST TO HARDEN ST) ROAD DIET COMMENTS 11/7/18

#	Comment
1	<p>I serve as the co-chair of BPAC, and I assisted with canvassing around Assembly Street to notify owners/occupants about the meeting. I generally got neutral or positive feedback; many business owners described being able to hear accidents at the intersection several times a day and were appreciative of this proved strategy to fix it. The only request/ comment came from an avid biker who asked for the bike lane to extend the entire stretch for consistency.</p> <p>I am very supportive of a road diet on this road for many reasons: improved safety for all travel modes, potential for business and economic development in the area. Please continue to move forward with your design. There are so many lot and side streets that could be shared by businesses if they have concerns about lost parking. We really hope more people will walk and bike to this area anyway!</p> <p>Also-this project will slow down traffic and bring safety to the sidewalks too. I think more ‘eyes on the street’ from foot traffic can also alleviate those types of concerns.</p>
2	Very supportive of the project. Would love to see more investment across city for similar projects. Ideally, would be better to have bike lane between parking and sidewalk- but can’t always get what you want.
3	I’m happy bike lanes are being done in the city! I’d love for Cola to become more bike/walk friendly. And have them connected. My fear is the drug traffic that is extremely prevalent right now on Calhoun- especially the area of Calhoun and Marion street intersection. If people do not feel safe, they will not use these bike lanes. Also, many businesses either already have or are considering leaving the area. My hope would be that making the area more bike and walk friendly will improve the overall workability/ economic vitality of the area. Hopefully dollars well spent!
4	<ul style="list-style-type: none"> • Myself (and AgFirst) have <u>strong safety concerns</u> between Assembly and Main. People turning from Assembly into the parking garage at AgFirst is a major traffic flow. • Why didn’t we consider road diet between Assembly and Main/Sumter • No BPAC/ bicycle involvement or business involvement in design, this needs to change • Other areas are ok/ good • Are we using bollards to segregate the lanes?
5	I feel the design from Park to Assembly should be used for the Assembly to Sumter section- 3 lanes with tuning lane. Additionally the Gregg to Barnwell section is a 15%+ climb that is blind from Barnwell. There should be bike lanes there, not Sharrows!
6	<p>I am a huge supporter of this plan, with the exception of the Assembly to Sumter section. That road gets relatively little traffic and a road diet to make it three lanes (just like Park to Assembly) would make it tremendously safer. As it is being proposed people will inevitably drive through the bike lane as they turn into the AgFirst parking garage off of Assembly.</p> <p>I also think Gregg to Barnwell needs to be a bike lane, it’s a blind hill and 15%+ grade so cyclists will be moving slowly.</p> <p>Thank you for gathering input!</p>
7	Everything looks great except Sumter to Assembly because of the single side bike lanes. I am good with the one change of the sharrow close to Arsenal Hill.
8	Everything looks great! I support the project and am encouraged by the county’s commitment to making our community more bike friendly!

9	Only concern is our office tenant access to our parking garage and potential for injury. In am (7:30-8am) and pm (4-5:30pm) we have about 600-700 people turning onto or from Calhoun into alley road access from transitions. We are a bike friendly certified building and want to make sure the bikes are safely travelling by our property.
10	Oppose removing existing park spaces on the 1300 block of Calhoun. Need already for on premise customer parking. Homeless situation remains a problem- this building remained empty for four years. Current tenant has parking issues with zoning at this time.
11	<ol style="list-style-type: none"> 1. Please prioritize concerns of those of us who actually live in the neighborhood affected. 2. Would like to see much less sharrow space and more dedicated bike lane. A sharrow is just another road sign that drivers ignore. 3. In areas where parking must remain it would be great to have it separate the bike lane from the traffic lane to increase safety. 4. Would like to see more space dedicated to bike lane to increase safe buffer zone, and cause cars to drive slower. Timid driving is NOT a negative downtown. 5. Please consider burying all power lines while you're at it with roadwork. 6. Would love to see a lower speed limit implemented along the bike routes, and eventually downtown-wide. #TWENTYISPLENTY <p>I live, work, worship, and shop downtown. I walk or bike downtown every single day. I'm very bold in asserting the rightaway that cars are supposed to yield. I have no doubt that anyone who is less confident does not feel safe getting around many spots not in a car. Cars routinely speed down too wide streets and run red lights (why I think sharrows aren't worth the paint and labor cost), nearly striking bikes and walkers almost weekly that I see. Please make Cola a better place for those of us who live here, and in doing so inspire more growth. Please resist the urge to give in to the pressure from loud malcontents who come to our neighborhoods by car, then drive across the river to a different city, county, and tax base. Thank you.</p>
12	<p>I am opposed to the Calhoun Street changes for the following reasons:</p> <p>Safety: The Calhoun Street Assembly Street intersection is very heavily traveled and has lots of accidents. Having bicycles in this dangerous intersection will increase the likelihood of serious injury. Daily deliveries will have to park in the bike lane obstruction bikes making them repeatedly change lanes between the bike lane and regular auto traffic. Most of Calhoun is already a share the road situation adding some blocks of bike lane will make it confusing, no-bike lane, bike lane, no bike lane. The North side of the road has lots of driveways so autos will be crossing the bike lane more often causing safety issue as well as delays in travel.</p> <p>Traffic Slowdown: The number of autos going through the intersection will be reduced with fewer lanes.</p> <p>Lack of need: Very few bicycles travel at this location. Bicycle advocates have not been actively proposing this change.</p> <p>Parking Difficulties: Downtown parking is already an issue, people going to the courthouse or federal building and other businesses in the area will have fewer options.</p> <p>Suggestions: Consider a less traveled road like Richland for the bike lane.</p>
13	I understand there is a proposal to remove the parking spots from in front of the offices located on Calhoun street. As I use a wheelchair, there is no other parking available to me or my disabled clients and we cannot afford the removal of these spots. Please advise if there is any issue with my request to keep the parking spaces on Calhoun street.
14	I support the revised plan that does not eliminate parking on the north side of the 900 block of Calhoun St. The parking at St. Timothy's is limited because of the federal building, and losing any means space would be a burden. Most of our members are old (including me) and we would have to park further also, it would be more walking for our members coming for weddings, funerals, and other events. Thank you so much for recommitting this revision so that our parking on the north side of the 900 block of Calhoun can be proposed. St. Timothy's is deeply grateful.

15	<p>My wife and I have attended St. Timothy’s Episcopal for several years now. Plus our daughter was married at St. Tim’s last December. St. Timothy’s is a hidden gem, with many older patrons and some families with young children. While we think it’s great to put in bike lanes through the area, we believe taking parking spaces along the street would have a negative impact on the church! We certainly don’t want to lose more patrons because there is not enough parking within close walking distance of the church. We cannot afford to lose more people, as it would impact our donations, as well as possibly membership. And when we had our daughter’s wedding, both sides of the street and around the corner of that block were used (as well as church parking) as it was raining and cold we needed all the nearby parking to handle the 100+ guest. As quiet as that end of the street is, I feel certain we could share parking with the bike lanes and satisfy the planners and church goers: I’ve never seen bike riders in the area so would hate to lose that parking for church goers!</p> <p>Thanks</p>
16	<p>I am a member of St. Timothy’s Episcopal Church, located at 900 Calhoun Street. I am very grateful that the City of Columbia has taken the concerns of our congregation seriously to parking in its proposed “bike” plan for Calhoun St. Our congregation is older and needs to have parking near the church as our parking lot is too small to serve the whole congregation. Thank you for listening to our representatives and allowing us to keep those parking spaces on Calhoun. We are so appreciative. Sincerely</p>
17	<p>Dear Ms. Higgins: As a business owner with a building on Calhoun Street, I am very opposed to any elimination of parking on Calhoun Street. Parking for the federal courthouse and for the many businesses on Calhoun Street is already in very short supply.</p> <p>We pay taxes. We vote. Do NOT eliminate any parking on Calhoun Street. My building is on the corner of Calhoun and Lincoln Streets.</p>
18	<p>I agree with Ms. Valtorta. We pay taxes at the higher rate as commercial property owners. Also, the federal courthouse often monopolizes the spaces on Calhoun Street.</p>
19	<p>My husband and I are cyclists and we 100% approve the bike lanes on Calhoun and Hampton Streets.</p> <p>Please approve.</p> <p>Sincerely</p>
20	<p>Thank you for taking the time to talk with me yesterday and explain the proposed changes. At this time I am strongly opposed to the modifications as I understand them for the following reasons:</p> <p>Safety – lots of significant accident at the Calhoun. Assembly street intersection. They are auto to auto, auto to bicycle would be devastating</p> <p>The need is not present. Who is advocating for this change? Very very few bicycles travel at this location.</p> <p><u>Please provide any data you may have about bicycle traffic on Calhoun and at this intersection specifically.</u></p> <p>The only bicycle advocates I talked with at the meeting said they were not consulted and he didn’t think it was the best plan. Clearly that is just one opinion.</p> <p>Downtown parking is already an issue, we ask people to leave our parking lot daily that are going to the courthouse or federal building.</p> <p>I don’t see how the number of autos going through the intersection would not be adversely effected, increasing the daily travel time for hundreds or thousands of people.</p> <p><u>Please provide any studies and information available on how the proposed changes impact traffic flow.</u></p>

	<p>Daily deliveries at just my building, UPS, FedEx, Mail, Shredding services etc. park on road and would obstruct the bike lane adding to the safety concerns. Bicycles would have to repeatedly change lanes between the bike lane and regular auto traffic.</p> <p>Most of Calhoun is already a share the road situation adding about 6-8 blocks of bike lane will make it confusing no-bike lane, bike lane, no bike lane. Once again safety concerns</p> <p>The North side of road has lots of driveways so autos will be crossing the bike lane more often causing safety issue as well as delays in travel. If the bicycle volume is so low as to not cause a safety/ delay issue then why commit the resources to have the bicycle lane?</p> <p>The office buildings at the Calhoun/Assembly intersection have not been fully occupied. Now new tenants are moving in you will see more traffic and on street parking.</p> <p>8 parking spaces will be removed on just my block</p> <p>Suggestions</p> <p>Why not move the bicycle path to Richland it has a lot less traffic.</p> <p>The proposal will reduce parking along Calhoun Street. If the goal it to maximize the utilization of this public asset make the parking free and you will have the spaces occupied and benefit hundreds of people vs the very few bicycle riders for the same cost.</p> <p>I do see a potential benefit in reducing the number of autos in downtown Columbia. But before making it more difficult for autos to get in an out of Columbia an alternative needs to be available. Having a convenient, cost-effective mass transit system to get people from where they live to downtown so they don't have to drive is the first step. Once in Columbia they can use bicycles etc.</p> <p><u>Please provide contact information for others I need to contact about my concerns.</u> I would like to meet with them quickly so I and take action with others in the community to stop this change before it progresses any further.</p> <p>Thanks again for your time.</p>
21	<p>Hi! As a business owner in Columbia, I strongly support bike lanes on Hampton and Calhoun. The more livable our city, the more attractive we are to business site selection teams, etc.</p>
22	<p>To Whom It May Concern:</p> <p>I am a major supporter of the expansion of bicycle lanes in Columbia and Richland County. There are few local/municipal improvements that would have so immediate and direct an impact on the quality of my day-to-day life than the Hampton-Calhoun lane project.</p> <p>I commute by bicycle every weekday from the Wales Garden neighborhood to an office on the corner of Sumter and Calhoun, and I hope you'll move forward with the project.</p>
23	<p>I am writing to you in support of the Calhoun Street bike lane project. I canvassed businesses for about 5 hours informing them of the public meeting and discussing the bike lane with them. The only negative comments I received are addressable. The eye clinic (Calhoun) said people leave with blurry vision and Northwestern Mutual (Bull/Richland) said people run the red light and accidents happen. I noticed there are no cross walks nor is there a cross light to assist walkers crossing Richland at Bull on either side of the street. Everyone else was very positive.</p>

<p>24</p>	<p>To Whom It May Concern:</p> <p>While I am a not a Richland resident, living just over the Gervais Bridge in West Columbia, the city of Columbia is an exciting place that my family loves to frequent. While we certainly enjoy the ever growing trails and paths for walking and biking within Richland county, we also bike on surface roads to Main Street to enjoy Soda City, to the Richland Library for meetings and story times, and we often bike to the state house for events. I bike over to Harden for pet supplies, and to Huger Publix for groceries. Having a safer access thru Hampton and Calhoun thru a "road diet" would be great, and could extend so many people's access to safe biking.</p> <p>For most of the year, minus the coldest of winter and the hottest of summer, our area is great for bikes. Everyone knows that parking is in short supply on downtown streets, and the cars don't go much faster than the average bike in the traffic downtown. Having more access for alternate modes of transportation, including bikes, ebikes, pedestrian paths, and in general making our area feel user friendly and welcoming is nothing but a good thing. Encouraging more drivers to find one of the many garages, or even ditching the car and riding a bike, or take the bus are all things that can help tremendously with the congestion people incur downtown on a regular basis.</p> <p>Thank you for considering the various modes of transportation and their access to the roads to help continue towards progress within Richland County.</p>
<p>25</p>	<p>Good Evening, Columbia needs well designed and safe bike lanes. Please include bike lanes downtown in any future infrastructure upgrades downtown. the city would benefit greatly form lanes in the Calhoun and Hampton street area.</p>
<p>26</p>	<p>I strongly support the changes proposed that will implement bike lanes on Calhoun and Hampton. Please add bike lanes for as much of the distance as possible, rather than sharrows where this is an option. Separated bike lanes give much safer travel for cyclists than sharrows. I encourage the county to over-communicate this change in the area to avoid the backtracking that was done on Farrow Road earlier. Cyclists deserve safe roads, and this will give two important routes connecting downtown to Harden.</p> <p>I'm a cyclist living in Rosewood and am encouraged to see more safe biking routes in town.</p>
<p>27</p>	<p>Hello,</p> <p>I was just made aware of the public meeting on June 28, 2018 which covered in part the proposed bike lane improvements for Hampton and Calhoun streets. My wife and I would like to be on record as being very much in favor of using some of the money already collected from the penny tax for these bike lanes. The proposed design looked very good from what we saw in the report.</p> <p>We are bicycle enthusiasts and enjoy riding on city trails and bike lanes, so please consider moving this project forward this year if at all possible. If you can keep us up to date on this matter somehow, perhaps even via an email list, we would greatly appreciate it.</p> <p>Thank you.</p>
<p>28</p>	<p>Hi,</p>

	<p>I am writing in favor of the proposed road diet on Calhoun and Hampton street, and especially the increased bike infrastructure. I regularly ride my bike on these streets and feel it would greatly benefit the safety of both bikes, pedestrians, and cars.</p> <p>Thanks,</p>
29	<p>Hello,</p> <p>I just wanted to write and let you know of my support for the proposed road diet on Hampton and Calhoun streets. As a current resident of downtown Columbia this will be a step forward in creating transportation equity for residents throughout the downtown area and throughout the city. As a former resident of Madison, WI (one who couldn't afford a car at the time), the bike lane infrastructure and bike culture of that city was invaluable in allowing me to safely travel throughout downtown and access all needed parts of the city without having to have a car. I would love to see Columbia get to that point in the near future, and I believe that with improvements such as bike lanes, trails, and education, we can. I urge you to move this project forward, and to continue supporting projects such as these for residents.</p> <p>Thanks</p>
30	<p>To all,</p> <p>This is a vote of support for moving forward on Calhoun and Hampton bike projects.</p> <p>I love biking in Columbia. I live downtown, work at Palmetto Health / USC in internal medicine, and bike to work as well. Moving these biking infrastructure projects forward is hugely important for many reasons - biking safety, reduce traffic, QOL, recruitment of residents, business opportunities, etc. I travel frequently and see other communities well ahead of us. I hope we can catch up!</p> <p>I serve in multiple capacities within the Palmetto Health ecosystem as well as on the city's committee Climate Protection Action Committee. Happy to collaborate in any way in the future.</p> <p>Thanks,</p>
31	<p>Hello,</p> <p>As a resident of downtown Columbia, I am writing to express my approval of the plan for bike infrastructure on Hampton and Calhoun. I both drive and ride my bike, and I think this plan is a great compromise between improving bicycling safety and still being convenient for drivers as well.</p> <p>I do highly encourage you to make sure to publicize the changes widely to avoid confusion and frustration on the part of drivers and to let cyclists know of these changes so they may utilize the lanes when planning their routes around town.</p> <p>Thank you!</p>
32	<p>Dear Richland Penny,</p> <p>I'm writing to express my support for the plan to add bike lanes to Calhoun and Hampton Streets. As a resident of downtown and a local cyclist, I think this is a much-needed improvement to Columbia's infrastructure. Thanks so much for seeing it to fruition.</p>

	Best,
33	<p>To whom it may concern,</p> <p>I travel Hampton Street five days a week to work and am highly in favor of adding protected bike lanes.</p> <p>Kindest regards,</p>
34	<p>I am in favor of putting bicycle lanes all over the Midlands, as vehicle traffic is not an option for all of our citizens. As a combat veteran who was stationed in Europe for four years, I have seen firsthand the benefits of innovative and progressive infrastructure. It improves employment, reduces accidents, and offers better opportunities for success. Transportation is a crucial factor in producing successful communities, and providing that access opens a floodgate.</p> <p>I likewise understand that education to the public is vital to its success, and there are a plethora of local resources, organizations and daily commuters who can provide that information. As a avid cyclist who totes my one year old in tandem, I understand the challenges involved in safety. Please feel free to contact me with any questions about how to grow our city, county, state and nation into a global transportation leader.</p>
35	<p>I went to the recent Calhoun Street road diet update meeting. I took one of the images from the website and modified it for what I believe is a plausible better alternative to 48' blocks there and around the city.</p> <p>This design:</p> <ul style="list-style-type: none"> • Separates bike lane from traffic with parking for more protection • still increases car lane width from existing, but narrows it from proposed to help reduce car speeds • allows the block to keep about 40% of parking • maintains a left turn lane • provides an additional 8' zone that could be used for a variety of uses (e.g. rain swales, bike/scooter parking, parking payment kiosks, BlueBike stations, public art, etc.) <p>The concept of the travel lane turning into a left turn lane and straight/right lane drifting to the right into what was parking space earlier in the block (no idea the technical term) is already in use in the city elsewhere (e.g. Pendleton Street between Marion and Sumter) and seems to work well.</p>
36	<p>Hi there,</p> <p>I am emailing because I saw the proposed bike lanes on Hampton and Calhoun and am really excited about the possibility of those being put in. I regularly bike along those streets and having a protected bike lane would make me feel much safer and more comfortable.</p> <p>Thank you for your time and consideration.</p> <p>Warm regards,</p>
37	<p>Hello,</p> <p>My name is Jonathan and I absolutely support the Calhoun St road diet. Columbia needs to be proactive in redesigning its streets in a smart way with the future in mind. Columbia's roads are hazardous, obsolete and</p>

	<p>need to better accommodate for busses, cyclists and pedestrians. Unless Columbia wishes to convey a lack of economic power, livability, and adaptability we NEED to do better.</p> <p>As deadly as it is (especially in South Carolina), I primarily get around by bicycle. I will continue to do so, at all costs, including my life. However, other South Carolinians deserve to be free of this burden. Therefore it is imperative South Carolina learns from the outside world, unless it wishes to be isolated and left behind.</p> <p>Thank You.</p>
38	<p>I absolutely support the placement of bike lanes as an improvement to Hampton and Calhoun St. Bike lanes are significantly more important than parking spots, especially because the ones on Hampton and Calhoun often remain empty! This often empty space would better serve the community if properly utilized.</p> <p>This would help Columbia become economically attractive to the rest of the country and world, where the bicycle is a common and affordable mode of transportation.</p>
39	<p>I say yes to bike lanes on Hampton and Calhoun</p>
40	<p>To Whom it May Concern,</p> <p>My bicycle is my sole mode of transportation. Making the shoulders of Shop Road wider or even better, into bike lanes, would help make what is a life-threatening trip for myself and other travelers safe. I still risk my life and ride my bicycle on Shop road to make the journey. I will continue to do so as long as I live here, and as long as I have friends and family to visit. 2 foot wide shoulders is not enough.</p> <p>Adding bike lanes or generous shoulders would connect Southern Richland county to Downtown. That's what this is about. This is about more livable communities for future generations, improving our public spaces for more face to face interaction. This is about a low-income person riding or walking to work to have a shot at a better life for themselves and their children.</p> <p>Improving infrastructure and livability is a non-partisan issue for a better future, and many people want to be able to enjoy their neighborhoods like they could back in the day. Outside, enjoying other people, nature, supporting nearby local business and not trapped in a car in traffic slowly resembling a busy city's.</p> <p>Thank you for your time.</p>
41	<p>Hi there,</p> <p>I absolutely support the implementation of bike lanes along Hampton and Calhoun st. I also support bike lanes wherever they may and should be placed.</p> <p>Hopeful,</p>
42	<p>Dear Mayor Benjamin:</p> <p>My law partner Bo Willard and I are the owners of 1002 Calhoun Street, LLC which owns the building and property on the southeast corner of Park and Calhoun Streets. We operate our law firm, Montgomery Willard, LLC on the premises. As a longtime Calhoun Street property owner and operator of a business on the street, traffic and the proposed diet plan are of great concern relating to me. As you are well aware, the property owners in this area already deal with many problems relating to the unaddressed homeless problem with in the City. We believe that the proposed diet plan, will not improve traffic, parking or any other issue</p>

that we are experiencing. To the contrary, it is my view that this proposal will have negative impacts and is an effort to address a non-existent problem. The potential new problems include but are not limited to:

- The reduction in lanes crossing Assembly will cause more of a traffic backup on Calhoun
- Reduced parking will make it more difficult to access businesses.
- Safety of bikers crossing Assembly, which is a high accident intersection.
- Daily UPS, FedEx, USPS, and other delivery services will use the bike lane causing bikers to weave in and out of traffic.
- The final plan has significant “share the road” requirements (no bike lane for much of Calhoun Street) confusing the public and cyclists as to where bike lanes are available.

Along with these presented problems, very few bikers are currently riding in this area. There are weeks where we don't observe a single cyclist on the street. When we do see cyclist, they are generally traversing Park Street and not Calhoun. I do not believe that the proposed plan will be an efficient use of tax monies and from our vantage point, it will create new problems in an effort to solve a nonexistent problem. We believe that it may have a negative impact on property values and that tax monies would serve our neighborhood better if they were used to address the homeless crises and support the COMET. Please allow this letter to express my opposition to the plan and that of my partner and employees as well. I would respectfully request that you and members of the council consider the opinions of the property owners and businesses in the area. I understand that opposition to this plan is practically universal. If you have any questions about my concerns, please do not hesitate to contact me. I am copying all members of council so that each is aware of my opposition. Thanks to you and each of the members of City Council for your service, work and consideration. I understand and appreciate the time and commitment that you spend on these crucial issues.



TRANSPORTATION PROGRAM

Richland County Transportation Program

Road Diet Concept Report

Calhoun Street (Wayne St to Harden St)



May 2018

I. Executive Summary

The Richland County Transportation Program has a total funding of \$1.07 billion funded through the Transportation Sales Tax approved by voters in November of 2012. Per the referendum, \$300,991,000 is dedicated to Transit with the remaining \$769,009,000 dedicated to the categories of Administration, Bike/Ped/Greenway, and Roadway. Of the \$80,883,356 designated for Bike/Ped/Greenway, \$22,008,775 was allotted for the development of bikeway projects throughout the County to enhance recreation and provide alternative modes of transportation.

A total of 87 bikeway projects were included in the referendum. These have been identified and categorized into four groups for development - 8 Shared-Use Paths; 39 Bike Lanes; 26 Signs and Sharrows Routes; and 14 Widening projects. Two of the Shared-Use Paths are currently being constructed. The Richland Program Development Team (PDT) is coordinating implementation of the 29 Signs and Sharrows routes with the City and SCDOT. Ten of the 14 bikeways included in the Widening projects are either in construction or are being designed. The remaining 4 Widening projects are scheduled to begin design in late 2018.

The 39 Bike Lanes group includes methods of development that involve Road Diet studies and opportunities for Re-Striping. Within the 39 Bike Lanes group, 5 are completed; 2 are under construction; 7 are in design; 17 are considered for restriping; and, 2 may be deleted due to safety concerns – for a total of 33 projects.

This report focuses on one of the remaining 6 that are scheduled for studies as Road Diets, Calhoun Street from Wayne Street to Harden Street. The PDT has developed this report through discussions with the City to coordinate a road diet plan that meets generally accepted requirements for bike lanes. The information in this report includes requirements for city bike lanes, existing conditions, and alternates for striping to accommodate bike lanes.

It is recommended that a sharrow be used for the 33' sections of Calhoun (from Wayne to Lincoln and from Pickens to Harden) with no other changes to parking or lane widths. A sharrow route shows cyclist a preferred route and informs motorist to share the road with cyclist. For the 48' sections of Calhoun (from Lincoln to Assembly and from Sumter to Pickens), it is recommended that the four through lanes be reduced to three lanes (one lane in each direction with a center lane for left turns) and remove parking along the north side of Calhoun to provide bike lanes in both directions. Along the 62' sections of Calhoun (Assembly to Sumter), the lane widths will be reduced to 11.5'/12' lanes and parking removed from the north side to allow for bike lanes in both directions. The above roadway widths do not include the width of gutter. Details of the above recommendations are provided in Section IV, Alternate 1 of this report.

II. Requirements for City Bike lanes

Per National Association of City Transportation Officials (NACTO)

The desirable bike lane width adjacent to curb face is 6 feet with a minimum width of 3 feet. When placed adjacent to a parking lane, the desirable reach from the curb face to the edge of the bike lane (including the parking lane, bike lane and optional buffer between them) is 14.5 feet; the absolute minimum reach is 12 feet. A bike lane next to a parking lane shall be at least 5 feet wide unless there is a marked buffer between them. Wherever possible, minimize parking lane width in favor of increased bike lane width. A solid white lane line marking shall be used to separate motor vehicle travel lanes from the bike lane. Most jurisdictions use a 6 to 8 inch line.

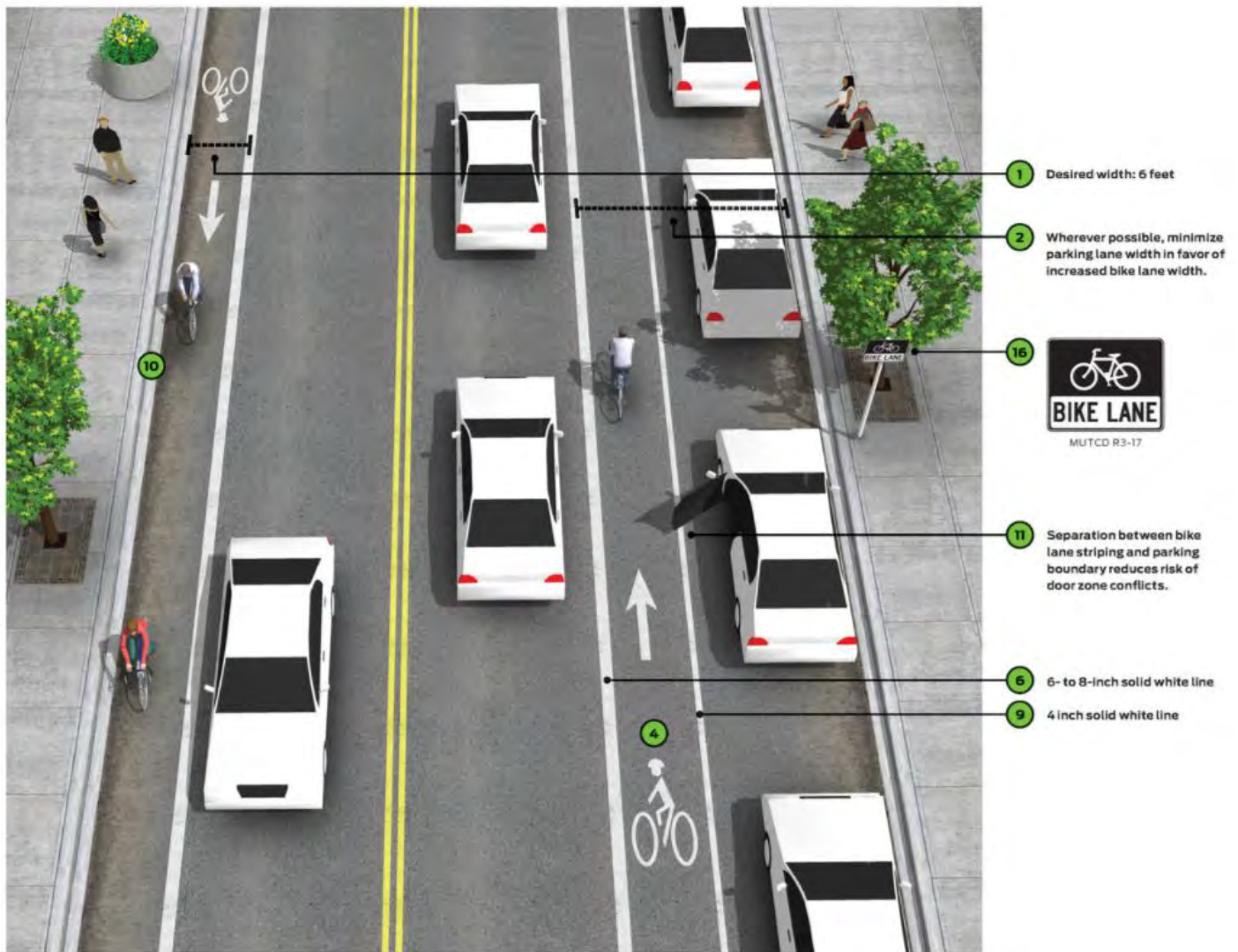


Photo courtesy of NACTO Urban Design Guide.

When placed adjacent to parking, a solid white line marking of 4-inch width should be used between the parking lane and the bike lane to minimize encroachment of parked cars into the bike lane. Gutter seams, drainage inlets, and utility covers should be flush with the ground and oriented to prevent conflicts with bicycle tires. Lane striping should be dashed through high traffic merging areas. The desirable dimensions should be used unless other street elements (e.g., travel lanes, medians, median offsets) have been reduced to their minimum dimensions. In cities where local vehicle codes require motor vehicles to merge into the bike lane in advance of a turn movement, lane striping should be dashed from 50 to 200 feet in advance of intersections to the intersection. Different states have varying requirements. “Bike Lane” signs (MUTCD R3-17) may be located prior to the beginning of a marked bike lane to designate that portion of the street for preferential use by bicyclists. The 2009 Manual on Uniform Traffic Control Devices (MUTCD) lists bike lane signs as optional; however, some states still require their use. On bike lanes adjacent to a curb, “No Parking” signs (MUTCD R8-3) may be used to discourage parking with the bike lane.

The recommendations in the report also reflect requirements of Cleveland Complete and Green Streets Typology Manual, Raleigh Street Design Manual, Charlotte Urban Street Design Guidelines and other related development standards.

III. Existing Conditions

Calhoun from Wayne to Lincoln is 33’ wide with two travel lanes and 6’ parking along on both sides (excluding gutter). Per SCDOT, this section has a low amount of traffic demand compared to the rest of the route with only 950 cars traveled per day counted at the Wayne and Calhoun intersection. The only available Average Daily Traffic Counts available on Calhoun are at the Wayne and Henderson intersections. The intersection of Wayne and Calhoun is also in close proximity to the Lincoln Tunnel Greenway providing opportunity for connectivity.

Calhoun from Lincoln to Assembly is 48’ wide with two 18’ travel lanes and 6’ parking on both sides of the road (excluding gutter).

Calhoun from Assembly to Sumter is 62’ wide with four 12’/13’ through lanes and 6’ parking both sides (excluding gutter). The current lane widths for this section have sufficient width to safely accommodate traffic.

Calhoun from Sumter to Pickens, is also 48’ wide but has four 10’ to 14’ travel lanes and only 3 parking spots on the north side. Due to the narrow travel lanes along Calhoun from Sumter to Pickens, it is uncommon that two vehicles travel in the same direction without one vehicle traveling a full vehicle length behind the other due to the potential, or driver concern, that side-swipe accidents may occur. Additionally, vehicles traveling in the outside lane often encroach into the inside lane out of concern for the proximity of parked vehicles. The narrow lane widths results in this section of Calhoun effectively functioning as a 3-lane roadway (1 travel lane in each direction and a continuous two-way left turn lane).

Calhoun from Pickens to Barnwell is 33' wide with one 13' and one 20' travel lane and no metered parking but unmarked parking does exist (excluding gutter). The unmarked parking is only on the south side and used for by residents occupying the nearby houses.

Calhoun from Barnwell to Harden is 33' wide with two 11' travel lanes and a two-way left turn lane (excluding gutter). The three lanes at 11' each take up all of the road width and leave no room for parking. The end of this route will connect to the Harden Street bike lane and provide connectivity. See the appendix for pavement marking details of the above.

Calhoun Parking Limits / Widths	North			South		
	Metered	Handicap	Other	Metered	Handicap	Other
Wayne-Gadsden (33')	0	0	8 (2Hr Park)	0	0	10 (2 Hr Park)
Gadsden-Lincoln (33')	8	0	0	12	0	0
Lincoln-Park (48')	9	1	0	2	0	10 (FED)
Park-Assembly (48')	8	0	0	8	0	0
Assembly-Main (62')	2	0	0	12	0	0
Main-Sumter (62')	11	0	0	12	0	0
Sumter-Marion (48')	0	0	0	0	0	0
Marion-Bull (48')	0	1	2 (No meter)	0	0	0
Bull-Pickens (48')	0	0	0	0	0	0
Pickens-Henderson (33')	0	0	0	0	0	0
Henderson-Barnwell (33')	0	0	0	0	0	0
Barnwell-Gregg (33')	0	0	0	0	0	0
Gregg-Harden (33')	0	0	0	0	0	0
Total	38	2	0	46	0	0

*Alternate 1 recommendation removes parking from Lincoln to Sumter totaling 30 spots removed.

Speed Limit: 35 MPH

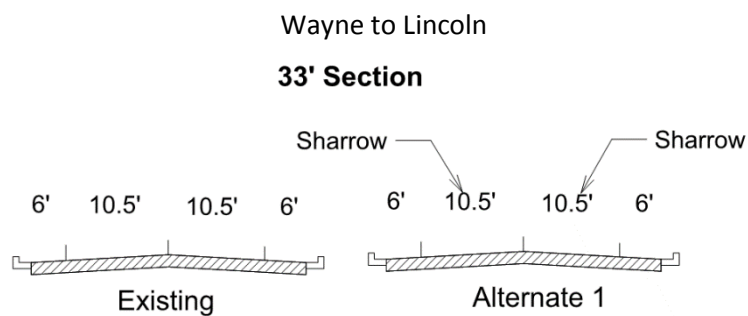
Average Daily Traffic: 950 (Calhoun Wayne Intersection) -6600 (Calhoun Henderson Intersection)

IV. Recommendation for Calhoun St.

In discussions with the City on parking removal and bike lane implementation, the conclusion was that removing parking on the same side for the entire route would be the safest and easiest for travel since it will prevent lane shifts at intersections. The City and PDT agreed that the north side parking would be more desirable to remove due to existing businesses on the south side.

Alternate 1

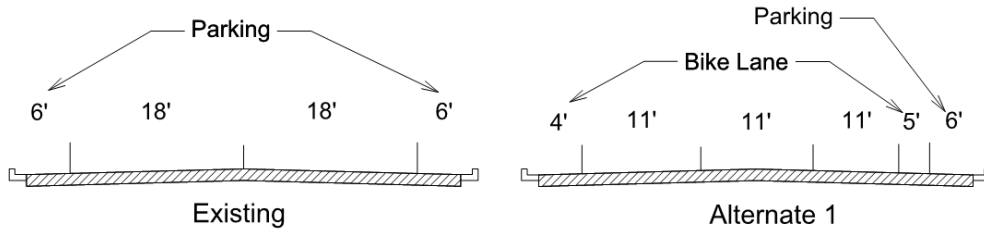
The section of **Calhoun from Wayne to Lincoln** is 33' wide (plus 1' of gutter on each side) with 2 travel lanes and parking on both sides. Through discussions with the City, it was determined that the only feasible way to accommodate bikes in this section of Calhoun was a sharrow. This is due to local businesses and federal parking. Sharrow symbols and signage would be applied and the existing lane configuration would not change. Refer to the below typical section for existing and alternate lane configuration for the remaining sections of Calhoun.



The section of **Calhoun from Lincoln to Assembly** is 48' wide (plus 1' of gutter on each side) with 2 travel lanes and parking on both sides. It is recommended that a two way left turn lane be added to create three lanes (a through lane in each direction with a median for left turns) and parking be removed from the north side of Calhoun to accommodate the bike lanes in both directions. This scenario would provide dedicated bike lanes in each direction and increased lane widths with no anticipated decrease in traffic capacity. The loss of these spaces would require additional parking spaces, but available parking spaces are located generally within 1-3 blocks on either side of Calhoun. Refer to the typical 48' sections below for existing and alternate lane configurations.

Lincoln to Assembly

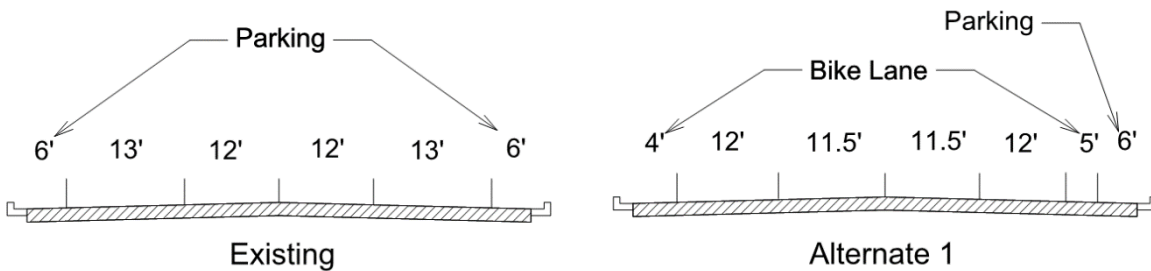
48' Section



For the 62' section of **Calhoun from Assembly to Sumter** which has four travel lanes and parking on both sides, it is recommended that parking be removed from the north side of Calhoun with reduced lane widths to allow for bike lanes in both directions. It is not recommended to remove a lane for this section because there is already enough existing width to restripe for a bike lane and four lanes will help support a higher volume of traffic. Refer to the typical 62' sections below for existing and alternate lane configurations:

Assembly to Sumter

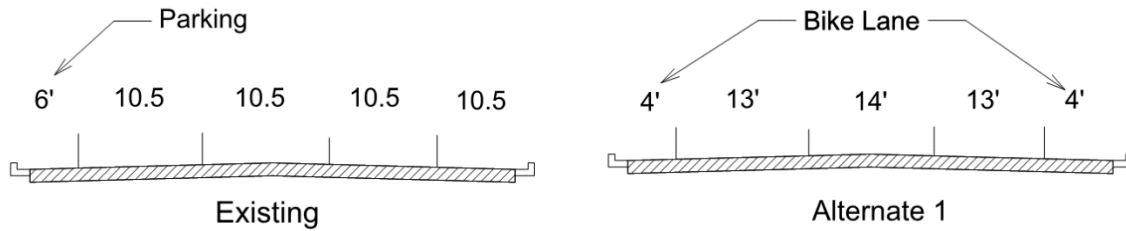
62' Section



The section of **Calhoun from Sumter to Pickens** is 48' wide (plus 1' of gutter on each side) with 4 travel lanes and parking on the north side. It is recommended the four lanes be reduced to three lanes (a through lane in each direction with a median for left turns) and parking be removed from the north side of Calhoun to accommodate the bike lanes in both directions. This scenario would provide dedicated bike lanes in each direction and increased lane widths with no anticipated decrease in traffic capacity. The loss of these spaces would require additional parking spaces, but available parking spaces are located generally within 1-3 blocks on either side of Calhoun:

Sumter to Pickens

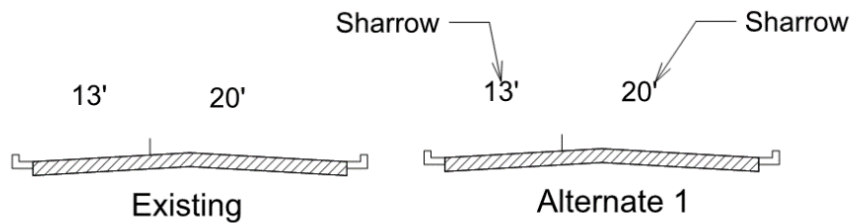
48' Section



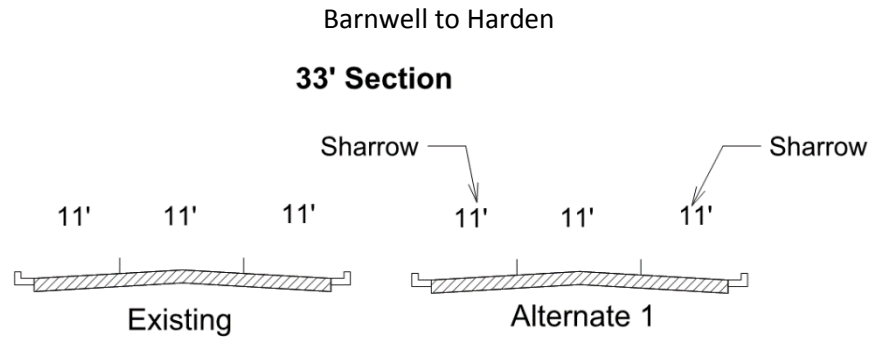
The section of **Calhoun from Pickens to Barnwell** is 33' wide (plus 1' of gutter on each side) with 2 travel lanes and on street but unmetered parking on the south side. Through discussions with the City, it was determined that the best way to accommodate bikes in this section of Calhoun was a sharrow which will allow for the unmarked parking to remain. Sharrows symbols and signage would be applied and the existing lane configuration would not change. Refer to the below typical section for existing and alternate lane configuration for the remaining sections of Calhoun:

Pickens to Barnwell

33' Section



The section of **Calhoun from Barnwell to Harden** is 33' wide (plus 1' of gutter on each side) with two 11' travel lanes and an 11' median travel lane. Through discussions with the City, it was determined that the only feasible way to accommodate bikes in this section of Calhoun was a sharrow. This was due to the existing median, lack of available width and anticipated traffic increase due to new development. Sharrows symbols and signage would be applied and the existing lane configuration would not change. Refer to the below typical section for existing and alternate lane configuration for the remaining sections of Calhoun:

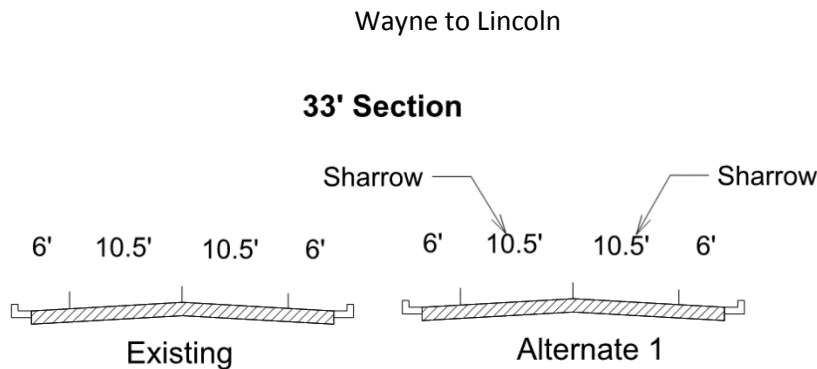


A total of 30 metered parking spaces would need to be removed with this alternate.

Alternate 2 (Remove Parking on Both Sides)

Removal of parking on both sides of Calhoun from Lincoln to Pickens would not be a recommended option as this would result in the removal of 64 spaces or 34 more spaces as compared to Alternate 1. Additionally, the increased lane widths, compared to Alternate 1, would not substantially contribute to either reduced accidents or traffic capacity.

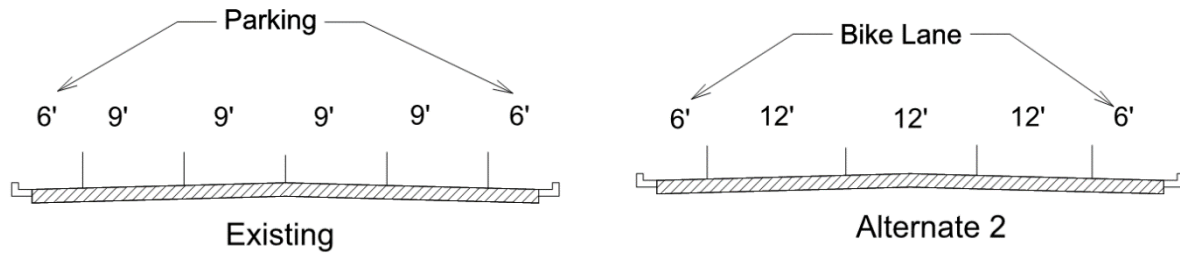
There is no recommendation for a second alternate for the 33' section of Calhoun from Wayne to Lincoln nor Pickens to Harden due to the narrow width:



Calhoun from Lincoln to Assembly would remove 27 metered parking spots, 10 more than removing just the north side. Refer to the below typical section for existing and alternate lane configuration for this section of Calhoun:

Lincoln to Assembly

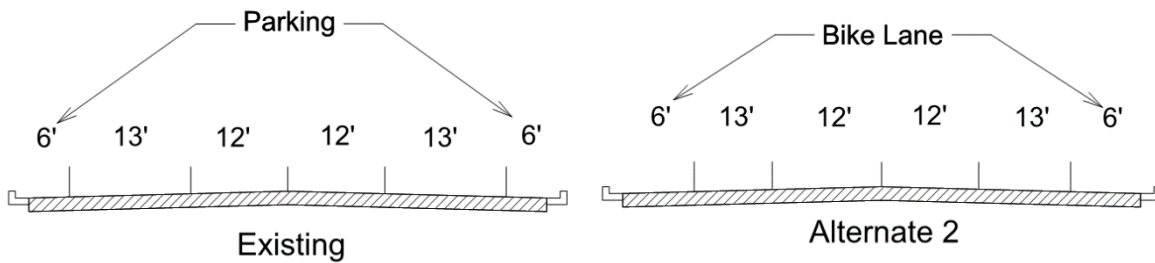
48' Section



Calhoun from Assembly to Sumter would remove 37 metered parking spots, 24 more than removing just the north side. Refer to the below typical section for existing and alternate lane configuration for this section of Calhoun:

Assembly to Sumter

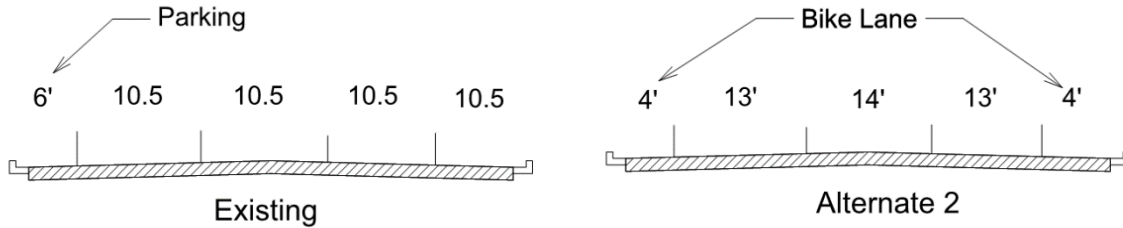
62' Section



Calhoun from Sumter to Pickens would not remove any more parking since there is no parking on the south side therefore there is no recommended alternate. Refer to the below typical section for existing and alternate lane configuration for this section of Calhoun:

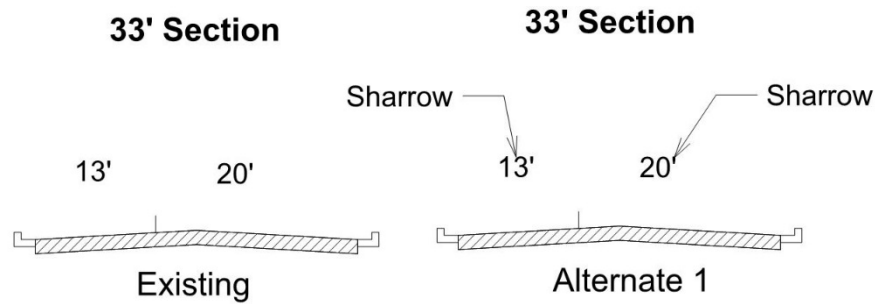
Sumter to Pickens

48' Section



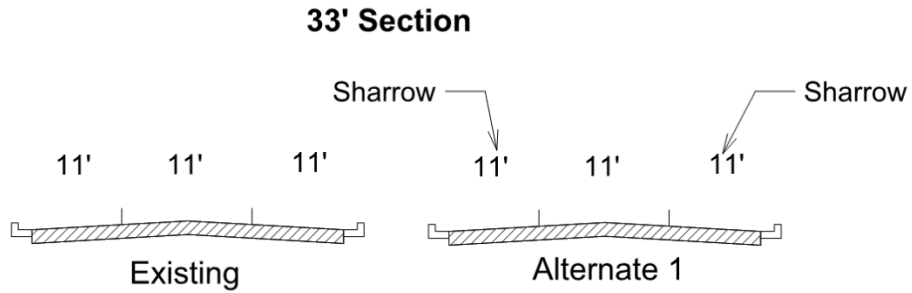
There is no recommendation for a second alternate for the 33' section from **Pickens to Barnwell** due to the narrow road width.

Pickens to Barnwell



There is no recommendation for a second alternate for the 33' section from **Barnwell to Harden** due to the narrow road width.

Barnwell to Harden



It is therefore recommended that Alternate 1 be implemented.

NOTE: See the appendix for existing striping along Calhoun Wayne to Harden.

February 4, 2019

The Honorable Calvin "Chip" Jackson
Chairman, Transportation Ad Hoc Committee
Richland County Council
2020 Hampton Street
Columbia, SC 29204

Dear Sir,

Jushi is nearing completion of its state-of-the-art manufacturing factory off Shop road Extension. We are excited about creating over 400 jobs in the local area. We currently have hired over 350 employee's year to date and will have over 450 employees by the time we start-up early next year. We appreciate the investment in the infrastructure like the shop road extension that will service our facility.

We have currently submitted an encroachment permit application and the turn lane permit application through DOT and awaiting their approval. Once approved, the additional turn lane, will take us (2) two months for us to construct, with an estimated completion date of March 31st.

We understand that the current deadline for opening the Shop Road extension is now March 1st, once Shop road extension is opened, it will cut off truck service to our site. For this reason, we respectfully request that you consider delaying the opening of Shop Road Extension until we complete the turn lane, on or around March 31st. Since we understand delaying the opening will result in additional inspector & administration cost for the county, we will up to \$30,000 (this is the county's estimated cost for inspector & administration cost) to delay the opening until March 31st. We ask you to consider this, because Jushi is the only tenant in the Pineview industrial park at this time and hence the delay will not significantly impact others.

Ray Wierzbowski

Jushi, VP of Operations

District: All
 Type: All
 Status: All

Program Status Report



No.	Project Name	Project Limits		District(s)	* Status
		From	To		
Widening					
271	Atlas Rd Widening (Q2 2019 Construction)	Bluff Rd	Garners Ferry Rd	10, 11	Right-of-Way Phase
425	Bluff Rd Widening Phase 1	Rosewood	George Rogers	10	Construction Complete
272	Bluff Road Phase 2 Improvements	National Guard	South Beltline Blvd	10	Design Phase
273	Blythewood Rd Widening (Q4 2019 Construction)	Syrup Mill Rd	I-77	02	Right-of-Way Phase
274	Blythewood Road Area Improvements	Fulmer Road	Main Street	02	Design Phase
275	Broad River Rd Widening	Royal Tower Rd	Dutch Fork Rd	01	Design Phase
276	Clemson Rd Widening	Old Clemson Rd	Chimneyridge Drive	09, 10	Construction Phase
277	Hardscrabble Rd Widening	Farrow Road	Kelly Mill Road	02, 07, 08, 09	Construction Phase
278	Leesburg Road Widening (Q4 2019 Construction)	Fairmont Rd	Lower Richland Blvd	10, 11	Right-of-Way Phase
279	Lower Richland Blvd Widening	Rabbit Run Rd	Garners Ferry Rd	11	Design Phase
280	North Main Street (Phases IA2 & III; II & IV) Widening	Anthony Avenue	Fuller Avenue	04	Construction Phase
281	Pineview Rd Improvements	Bluff Rd	Garners Ferry Rd	10, 11	Design Phase
282	Polo Rd Widening	Mallet Hill Rd	Two Notch Rd	08, 09, 10	Design Phase
283	Shop Rd Widening	George Rogers Blvd	South Beltline Blvd	10	Design Phase
284	Spears Creek Church Rd Widening	Two Notch Rd	Percival Rd	09, 10	Not Started
Intersection					
292	Broad River Rd. and Rushmore Rd. Intersection	Broad River Rd.	Rushmore Rd.	02	Construction Complete
293	Bull St. and Elmwood Ave. Intersection	Bull St.	Elmwood Ave.	04	Right-of-Way Phase
294	Clemson Rd. and Rhame Rd./North Springs Rd.	Clemson Rd.	Rhame Rd./North	08, 09	Construction Complete
295	Clemson Rd. and Sparkleberry Ln. (to Mallet Hill Rd.)	Clemson Rd.	Sparkleberry Ln.	09, 10	Right-of-Way Phase
296	Farrow Rd. and Pisgah Church Rd. Intersection	Farrow Rd.	Pisgah Church Rd.	07	Construction Complete
297	Garners Ferry Rd. and Harmon Rd. Intersection	Garners Ferry Rd.	Harmon Rd.	11	Right-of-Way Phase
298	Hardscrabble Rd. and Kelly Mill Rd./Rimer Pond Rd.	Hardscrabble Rd.	Kelly Mill Rd./Rimer	02, 09	Construction Phase
299	Kennerly Rd. and Coogler Rd./Steeple Ridge Rd.	Kennerly Rd.	Coogler/Steeple Ridge	01	Construction Complete
300	North Main St. and Monticello Rd. Intersection	North Main St.	Monticello Rd.	04	Construction Phase
301	North Springs Rd. and Harrington Rd. Intersection	North Springs Rd.	Harrington Rd.	08, 09	Procurement Phase
302	North Springs Rd. and Risdon Way Intersection	North Springs Rd.	Risdon Way	08, 09	Construction Complete
303	Screaming Eagle Rd. and Percival Rd. Intersection	Screaming Eagle Rd.	Percival Rd.	09, 10	Right-of-Way Phase
304	Summit Pkwy and Summit Ridge Dr Intersection	Summit Pkwy	Summit Ridge Dr	08, 09	Construction Complete
305	Wilson Blvd. and Killian Rd. Intersection	Wilson Blvd.	Killian Rd.	07	Construction Complete
306	Wilson Blvd. and Pisgah Church Rd. Intersection	Wilson Blvd.	Pisgah Church Rd.	07	Construction Complete

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No.	Project Name	Project Limits		District(s)	* Status
		From	To		
Special					
285	Commerce Drive Improvements Special	Royster Street	Jim Hamilton Boulevard	05, 10	Not Started
287	Kelly Mill Rd.	Hardscrabble Rd.	EJW Road	02, 09	Not Started
289	Riverbanks Zoo Pedestrian Bridge			05	Construction Complete
290	Shop Road Extension Phase 1	Pineview Road	Longwood Road	10	Construction Phase
324	Shop Road Extension Phase 2	Longwood Road	Garners Ferry Road	10, 11	Design Phase
Innovista					
319	Innovista 1 - Greene Street Phase 1	Gadsden Street	Assembly Street	05	Construction Complete
321	Innovista - Greene Street Ph 2 (Q2 2019 Construction)	Huger Street	Gadsden Street	05	Right-of-Way Phase
322	Innovista 3 - Williams Street			05	Not Started
Neighborhood Improvement					
330	Broad River Corridor Neighborhood Improvements			02, 04, 05	Design Phase
328	Crane Creek Neighborhood Improvements			04, 07	Design Phase
326	Decker Blvd/Woodfield Park Neighborhood			08	Design Phase
325	Broad River Neighborhood Improvements			04	Construction Phase
327	Candlewood Neighborhood Improvements			08	Construction Phase
318	Southeast Richland Neighborhood Improvements (Q1 2019 Construction)			11	Procurement Phase
329	Trenholm Acres / Newcastle Neighborhood			03	Design Phase
Dirt Road					
	42 Roads	<p style="text-align: center;">\$5M in Procurement (Q1 2019 Construction Start)</p> <p style="text-align: center;">*\$20M / \$45M Dirt Road Funds under Contract or Complete</p>			Planning Phase
	36 Roads				Design Phase
	66 Roads				Right-of-Way Phase
	13 Roads				Procurement Phase
	9 Roads				Construction Phase
	48 Roads				Construction Complete
	55 Roads				Indefinitely Delayed
Resurfacing					
	95 Roads	<p style="text-align: center;">\$7M in Procurement (Q1 2019 Construction Start)</p> <p style="text-align: center;">* \$29M / \$41.4M Resurfacing Funds under Contract or Complete</p>			Procurement Phase
	103 Roads				Construction Phase
	290 Roads				Construction Complete

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No.	Project Name	Project Limits		District(s)	* Status
		From	To		
Pedestrian Intersection Improvements					
101	Assembly St and Calhoun St Intersection			04	Construction Phase
102	Assembly St and Gervais St Intersection			04, 05	Construction Phase
103	Assembly St and Laurel St Intersection			04	Construction Phase
104	Assembly St and Washington St Intersection			04, 05	Construction Phase
105	Blossom St and Saluda Ave Intersection			05	Construction Phase
106	Broad River Rd and Bush River Rd Intersection			04, 05	Construction Complete
107	Devine St and Harden St/Santee Ave Intersection			05	Construction Complete
108	Elmwood Ave and Bull St Intersection			04	Construction Complete
109	Elmwood Ave and Park St Intersection			04	Construction Phase
110	Harden St and Gervais St Intersection			04, 05	Construction Complete
111	Huger St and Blossom St Intersection			05	Construction Complete
112	Huger St and Gervais St Intersection			05	Construction Complete
Greenway					
131	Columbia Mall Greenway	Trenholm (N of O'Neil)	Trenholm (S of Dent)	03, 08	Design Phase
134	Crane Creek Greenway B	Crane Creek A	Smith Branch	04	Design Phase
133	Crane Creek Greenway Section A	Monticello Road	Broad River	04	Design Phase
132	Crane Creek Greenway Section C (Crane Forest)	Peachwood Dr	Crane Creek	04, 07	Design Phase
135	Dutchman Blvd Connector Greenway	Broad River Rd	Lake Murray Blvd	02	Not Started
136	Gills Creek A Greenway	Ft. Jackson Blvd	Mikell Ave	06	Design Phase
137	Gills Creek B Greenway	Wildcat Creek	Leesburg Road	06, 10, 11	Planning Phase
138	Gills Creek North Greenway C	Trenholm Rd	Lake Katherine	06	Planning Phase
139	Lincoln Tunnel Greenway	Finlay Park/Taylor St	Elmwood Ave Bridge	04, 05	Construction Complete
140	Polo Rd/Windsor Lake Connector Greenway	Polo Road	Windsor Lake Blvd	08	Not Started
143	Smith/Rocky Branch Greenway A	Three Rivers Greenway	Clement Rd	04	Design Phase
142	Smith/Rocky Branch Greenway B	Clement Rd	Colonial Dr	04	Design Phase
141	Smith/Rocky Branch Greenway C	Downtown	Granby Park	05, 10	Design Phase
144	Three Rivers Greenway Extension Ph. 1	I-26 overpass	Columbia Canal Walk	05	Construction Phase
145	Woodbury/Old Leesburg Connector Greenway	Woodbury Dr	Old Leesburg Rd	11	Not Started
Sidewalk					
146	Alpine Rd Sidewalk	Two Notch Rd	Percival Rd	03, 08, 10	Right-of-Way Phase
147	Assembly St Sidewalk	Whaley St	Beltline Blvd	05, 10	Design Phase
148	Blossom St Sidewalk	Williams St	Huger St	05	Construction Complete
149	Blythewood Rd Sidewalk	I-77	Main St	02	Construction Complete
150	Bratton St Sidewalk	King St	Fairview	05	Construction Complete

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No.	Project Name	Project Limits		District(s)	* Status
		From	To		
151	Broad River Rd Sidewalk	Greystone Blvd	Broad River Bridge	04, 05	Construction Complete
152	Broad River Rd Sidewalk	Harbison Blvd	Bush River Rd	02, 04, 05	Planning Phase
153	Broad River Rd Sidewalk	I-26	Harbison Blvd	02	Planning Phase
154	Calhoun St Sidewalk	Gadsden St	Wayne St	04	Construction Complete
182	Capers Ave Sidewalk	S. Ravenel	S. Ott	05	Construction Complete
155	Clemson Rd Sidewalk	Longtown Rd	Two Notch Rd	07, 08, 09	Design Phase
156	Clemson Rd Sidewalk Ph. 1	Two Notch Rd	Percival Rd	09, 10	Procurement Phase
157	Colonial Dr Sidewalk	Harden St	Academy St	04	Construction Complete
158	Columbiana Dr Sidewalk	Lexington County Line	Lake Murray Blvd	02	Construction Complete
159	Fort Jackson Blvd Sidewalk	Wildcat Rd	I-77	06	Design Phase
160	Franklin St Sidewalk	Sumter St	Bull St	04	Construction Complete
161	Gervais St Sidewalk	450' west of Gist St	Gist St	05	Construction Complete
162	Gervais St Sidewalk	Gist St	Huger St	05	Construction Complete
163	Grand St Sidewalk	Shealy St	Hydrick St	04	Construction Complete
164	Harrison Road Sidewalk	Two Notch Rd.	Forest Dr.	03	Procurement Phase
165	Huger St Sidewalk	Blossom St	Gervais St	05	Planning Phase
166	Jefferson St Sidewalk	Sumter St	Bull St	04	Construction Complete
167	Koon Road Sidewalk	Malinda Road	Farmview Street	03	Construction Phase
168	Laurel St Sidewalk	Gadsden St	Pulaski St	04, 05	Construction Complete
169	Leesburg Rd Sidewalk	Garners Ferry Rd	Semmes Rd	10, 11	Right-of-Way Phase
170	Lincoln St Sidewalk	Heyward St	Whaley St	05	Construction Complete
171	Lower Richland Blvd Sidewalk	Rabbit Run Rd	Garners Ferry Rd	11	Design Phase
172	Lyon St Sidewalk	Gervais St	Washington St	05	Construction Complete
173	Magnolia St Sidewalk	Two Notch Rd	Pinehurst Rd	03	Construction Complete
174	Maple St Sidewalk	Kirby St	Gervais St	06	Construction Complete
188	Marion St Sidewalk	Whaley St	Airport Blvd	05, 10	Construction Complete
175	Mildred Ave Sidewalk	Westwood Ave	Duke Ave	04	Construction Complete
176	Park St Sidewalk	Gervais St	Senate St	05	Design Phase
177	Pelham Dr Sidewalk	Gills Creek Parkway	Garners Ferry Road	06	Construction Complete
178	Percival Road Sidewalk	Forest Dr	Northshore Rd	06, 08, 10	Right-of-Way Phase
179	Pinehurst Sidewalk	Harrison Road	Forest Drive	03	Construction Complete
180	Polo Rd Sidewalk	Mallet Hill Rd	Alpine Rd	08, 09, 10	Procurement Phase
181	Prospect Sidewalk	Wilmot Avenue	Yale	05	Indefinitely Delayed
183	School House Rd Sidewalk	Two Notch Rd	Ervin St	03	Construction Phase
184	Senate St Sidewalk	Gladden St	Kings St	05, 06	Construction Complete

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No.	Project Name	Project Limits		District(s)	* Status
		From	To		
185	Shandon St Sidewalk	Rosewood Dr	Heyward St	05	Indefinitely Delayed
186	Shandon St Sidewalk	Wilmot St	Wheat St	05	Indefinitely Delayed
187	Sunset Sidewalk	Elmhurst Road	River Drive	04	Design Phase
189	Tryon St Sidewalk	Catawba St	Heyward St	05	Construction Complete
190	Two Notch Rd Sidewalk	Alpine Rd	Spears Creek Church Rd	03	Planning Phase
191	Veterans Sidewalk	Coachmaker Road	Coatsdale Road	06, 11	Construction Complete
192	Veterans Sidewalk	Garners Ferry Road	Wormwood Drive	11	Construction Complete
193	Wayne St Sidewalk	Calhoun St	Laurel St	04, 05	Construction Complete
194	Wildwood Ave Sidewalk	Monticello Rd	Ridgewood Ave	04	Construction Complete
195	Wiley St Sidewalk	Superior St	Edisto Ave	10	Construction Complete
196	Windover St Sidewalk	Two Notch Rd	Belvedere Dr	03	Construction Complete
Bikeway					
197	Alpine Rd Bike Lanes	Two Notch Rd	Percival Rd	03, 08, 10	Right-of-Way Phase
199	Assembly St Bikeways	Blossom St	Rosewood Dr	05, 10	Design Phase
198	Assembly St Bikeways	Blossom St	Rosewood Dr	10	Design Phase
200	Beltline Blvd Bikeways	Forest Dr	Valley Rd	03	Design Phase
201	Beltline Blvd Bikeways	Rosewood Dr	Devine St	06	Design Phase
202	Beltline Blvd/Colonial Dr/Farrow Rd Bikeways	Harden St	Academy St	04	Design Phase
203	Beltline Blvd/Devine St Bikeways	Rosewood Dr	Chateau Dr	06	Not Started
204	Blossom St Bikeways	Assembly St	Sumter St	05	Design Phase
205	Blossom St Bikeways	Huger St	Assembly St	05	Not Started
206	Blossom St Bikeways	Williams St	Huger St	05	Construction Complete
207	Blythwood Rd Bikeways	Winnsboro Rd	Main St	02, 07	Design Phase
208	Bonham/ Devereaux/ Heathwood/ Kilbourne/	Blossom St	Fort Jackson Blvd	05, 06	Planning Phase
210	Broad River Rd Bike Lanes	Greystone Blvd	Broad River Bridge	04, 05	Design Phase
209	Broad River Rd Bikeways	Bush River Rd	Greystone Blvd	04, 05	Design Phase
211	Broad River Rd Bikeways	Harbison Blvd	Bush River Rd	02, 04, 05	Not Started
212	Broad River Rd/Lake Murray Blvd Bikeways	I-26	Harbison Blvd	02	Not Started
213	Bull St Bikeways	Elmwood Ave	Victoria St	04	Design Phase
214	Bull St/Henderson St/Rice St Bikeways	Wheat St	Heyward St	05	Planning Phase
215	Calhoun St Bikeways	Wayne St	Harden St	04	Design Phase
216	Catawba St Bikeways	Sumter St	Lincoln St	05	Planning Phase
217	Catawba St/Lincoln St/Heyward St/Tryon St/Williams St	Catawba St	Blossom St	05	Planning Phase
218	Chester St/Elmwood Ave/Wayne St Bikeways	Hampton St	Park St	04	Planning Phase
219	Clement Rd/Duke Ave/River Dr Bikeways	Main St	Monticello Rd	04	Planning Phase

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No.	Project Name	Project Limits		District(s)	* Status
		From	To		
220	Clemson Rd Bikeways	Brook Hollow Dr	Summit Pky	08	Design Phase
221	Clemson Rd Bikeways	Longtown Rd	Brook Hollow Dr	07, 08	Design Phase
222	Clemson Rd Bikeways	Summit Pky	Percival Rd	08, 09, 10	Not Started
223	College St Bikeways	Lincoln St	Sumter St	04, 05	Design Phase
224	College St/Laurens St/Oak St/Taylor St Bikeways	Greene St	Elmwood Ave	05	Planning Phase
225	Colonial Dr Bikeways	Bull St	Slighs Ave	04	Planning Phase
226	Columbiana Dr Bikeways	Lake Murray Blvd	Lexington County Line	02	Design Phase
227	Craig Rd Bikeways	Harrison Rd	Covenant Rd	03	Planning Phase
228	Decker Blvd/Parklane Rd/Two Notch Rd Bikeways	Two Notch Rd	Percival Rd	03, 08	Design Phase
229	Dutchman Blvd Bikeways	Broad River Rd	Lake Murray Blvd	02	Not Started
230	Edgefield St/Park St Bikeways	Calhoun St	River Dr	04	Planning Phase
231	Elmwood Ave Bikeways	Wayne St	Proposed Greenway	04, 05	Planning Phase
232	Fort Jackson Blvd Multi-Use Path	Devine St	N. Kings Grant Dr.	06	Design Phase
233	Garners Ferry Rd Bikeways	Rosewood Dr	True St	06, 11	Design Phase
234	Gervais St Bikeways	450' west of Gist St	Gist St	05	Planning Phase
235	Gervais St Bikeways	Gist St	Huger St	05	Planning Phase
236	Gervais St Bikeways	Park St	Millwood Ave	04, 05	Planning Phase
237	Gervais/Gladden/Hagood/Page/Senate/Trenholm/Webst	Millwood Ave	Beltline Blvd	05, 06	Planning Phase
238	Greene St Bikeways	Assembly St	350' west of Lincoln St	05	Construction Complete
239	Greene St Bikeways	Assembly St	Bull St	04, 05	Planning Phase
240	Greene St Bikeways	Bull St	Saluda Ave	04, 05	Planning Phase
241	Hampton St Bikeways	Pickens St	Harden St	04	Design Phase
242	Harden St Bikeways	Devine St	Rosewood Dr	05	Planning Phase
243	Heyward St/Marion St/Superior St Bikeways	Whaley St	Wiley St	05, 10	Planning Phase
244	Holt Dr/Superior St Bikeways	Wiley St	Airport Blvd	05, 10	Planning Phase
245	Huger St Bikeways	Blossom St	Gervais St	05	Design Phase
246	Huger St/Lady St/Park St Bikeways	Gervais St	Gervais St	05	Planning Phase
247	Leesburg Rd Bikeways	Garners Ferry Rd	Semmes Rd	10, 11	Design Phase
248	Lincoln St Bikeways	Blossom St	Lady St	05	Design Phase
249	Main St Bikeways	Calhoun St	Elmwood Ave	04	Planning Phase
250	Main St Bikeways	Elmwood Ave	Sunset Dr	04	Construction Complete
251	Main St Bikeways	Pendleton St	Whaley St	04, 05	Planning Phase
252	Oneil Ct Bikeways	Decker Blvd	Parklane Rd	03, 08	Construction Complete
253	Ott Rd Bikeways	Jim Hamilton Blvd	Blossom St	05, 10	Planning Phase
254	Pendleton St Bikeways	Lincoln St	Marion St	04, 05	Design Phase

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No.	Project Name	Project Limits		District(s)	* Status
		From	To		
255	Pickens St Bikeways	Washington St	Rosewood Dr	04, 05	Design Phase
256	Pickens St/Washington St/Wayne St Bikeways	Hampton St	Hampton St	04, 05	Design Phase
257	Polo Rd Bikeways	Two Notch Rd	640' south of Mallet Hill Rd	08, 09, 10	Design Phase
258	Rosewood Dr Bikeways	Bluff Rd	Garners Ferry Rd	05, 06, 10	Design Phase
259	Saluda Ave Bikeways	Wheat St	Greene St	05	Planning Phase
260	Senate St Bikeways	Sumter St	Laurens St	04, 05	Planning Phase
261	Shop Rd Bikeways	Beltline Blvd	Pineview Dr	10	Not Started
262	Sumter St Bikeways	Blossom St	Wheat St	05	Planning Phase
263	Sumter St Bikeways	Washington St	Senate St	04	Design Phase
264	Trenholm Rd Bikeways	South of Dent Middle	Decker Blvd	03, 08	Construction Complete
265	Two Notch Rd Bikeways	Alpine Rd	Spears Creek Church Rd	03, 07, 08, 09	Planning Phase
266	Two Notch Rd Bikeways	Head St	Albritton Rd	03	Construction Complete
268	Whaley St Bike Lanes	Lincoln St	Pickens St	05	Design Phase
267	Whaley St Bikeways	Lincoln St	Church St	05	Planning Phase
269	Wheat St Bikeways	Harden St	King St	05	Design Phase
270	Wheat St Bikeways	Sumter St	Assembly St	05	Construction Complete
113	Huger St and Greene St Intersection			05	Construction Complete
114	Huger St and Lady St Intersection			05	Construction Complete
115	Main St and Blanding St Intersection			04	Design Phase
116	Main St and Calhoun St Intersection			04	Construction Phase
117	Main St and Elmwood Ave Intersection			04	Construction Complete
118	Main St and Laurel St Intersection			04	Design Phase
119	Rosewood Dr and Beltline Blvd Intersection			05, 06	Construction Phase
120	Rosewood Dr and Harden St Intersection			05	Construction Phase
121	Rosewood Dr and Holly St Intersection			05	Construction Phase
122	Rosewood Dr and Kilbourne Rd Intersection			05, 06	Construction Phase
123	Rosewood Dr and Marion St Intersection			05, 10	Construction Phase
124	Rosewood Dr and Ott Rd Intersection			05	Construction Phase
125	Rosewood Dr and Pickens St Intersection			05, 10	Construction Phase
126	Two Notch Rd and Alpine Rd Intersection			03, 07	Construction Complete
127	Two Notch Rd and Brickyard Rd Intersection			08, 09	Construction Phase
128	Two Notch Rd and Decker Blvd/Parklane Rd			03	Construction Complete
129	Two Notch Rd and Maingate Dr/Windsor Lake Blvd			03	Construction Phase
130	Two Notch Rd and Sparkleberry Ln Intersection			09	Construction Phase

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